



# Department of Administrative Services

## Purchasing and Contracts

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Sabra Smith Newby, Chief Administrative Officer  
Adleen B. Stidhum, Purchasing Administrator



### CLARK COUNTY, NEVADA BID NO. 603283-14 THOMAS EDISON DRIVE, BRUCE WOODBURY DRIVE TO LAUGHLIN CIVIC DRIVE PULVERIZE AND PAVE

May 2, 2014

#### ADDENDUM NO. 1

#### INVITATION TO BID

1. The bid opening date of May 9, 2014 at 2:15:00 p.m. **remains unchanged.**

#### BID SCHEDULE

2. **Delete** the original Bid Form pages 4-1 through 4-5 **and replace** with the attached revised Bid Form pages 4-1 through 4-5.

#### SPECIAL PROVISIONS

3. Please incorporate the attached:  
Section 107-1(ADD-1) to 107-8(ADD-1) pages of text into the bid documents  
Section 302-1(ADD-1) to 302-6(ADD-1) pages of text into the bid documents  
Section 627-1(ADD-1) to 627-2(ADD-1) pages of text into the bid documents

#### DRAWINGS

4. **Drawing No. 04**, Added driveway on west side of Thomas Edison Dr.
5. **Drawing No. 14**, Added Sheet
6. **Drawing No. 14.1**, Added driveway, updated striping, signage, legend and drawing number
7. **Drawing No. 15**, Updated dimension and drawing reference
8. **Drawing No. 38**, Updated striping, legend and add signage
9. **Drawing No. 39**, Show existing sign

Except as modified herein all other bid specifications, terms, conditions and special provisions shall remain the same.

ISSUED BY:

SANDY MOODY-UPTON  
Purchasing Analyst

Attachment(s): Section 107-1 (ADD-1) to 107-8 (ADD-1)  
Section 302-1 (ADD-1) to 302-6 (ADD-1)  
Section 627-1 (ADD-1) to 627-2 (ADD-1)  
DWG-04, DWG-14, DWG-14.1, DWG-15, DWG-38, DWG-39

Cc: Kathleen Kingston, Public Works  
Cindy Beauchamp, Public Works

BOARD OF COUNTY COMMISSIONERS  
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# CLARK COUNTY, NEVADA

## BID FORM

BID NO. 603283-14

THOMAS EDISON DRIVE, BRUCE WOODBURY DRIVE TO LAUGHLIN CIVIC DRIVE PULVERIZE AND PAVE  
PWP NUMBER: CL-2014-205  
Revised Per Addendum No. 1

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(NAME)

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(ADDRESS)

### I, THE UNDERSIGNED BIDDER:

1. Agree, if awarded this Contract, I will complete all work for which a Contract may be awarded and to furnish any and all labor, equipment, materials, transportation, and other facilities required for the services as set forth in the Bidding and Contract Documents.
2. Have examined the Contract Documents and the site(s) for the proposed work and satisfied themselves as to the character, quality of work to be performed, materials to be furnished and as to the requirements of the specifications.
3. Have completed all information in the blanks provided and have submitted the following within this Bid:
  - a) Have listed the name of each Subcontractor which will be paid an amount exceeding five percent (5%) of the Total Base Bid amount.
  - b) Attached a bid security (in the form of, at my option, a Cashiers Check, Certified Check, Money Order, or Bid Bond in favor of the Owner in the amount of five percent (5%) of the Total Base Bid amount.
  - c) If claiming the preference eligibility, I have submitted a valid Certificate of Eligibility with this Bid.
4. I acknowledge that if I am one of the three apparent low bidders at the bid opening, and if I have listed Subcontractor(s) pursuant to NRS 338.141, I must submit Bid Attachment 2 within two-hours after completion of the bid opening pursuant to the Instructions to Bidders, forms must be submitted via hand delivery or email to [COUNTPURCHASING@CLARKCOUNTYNV.GOV](mailto:COUNTPURCHASING@CLARKCOUNTYNV.GOV) and I understand that hand delivery is recommended, and Owner shall not be responsible for lists received after the two-hour time limit, regardless of the reason. I understand that submission after the two-hour time limit is not allowed and will be returned to me and the bid will be deemed non-responsive. I acknowledge that for:
  - a) Projects UNDER \$5,000,000  
I need to list **only those Subcontractors** that will provide labor/improvements exceeding \$50,000.00.
5. I acknowledge that if notified that I am the low bidder, I must submit the Disclosure of Ownership/Principals form within 24-hours of request.
6. I acknowledge that my bid is based on the current State of Nevada prevailing wages.
7. I acknowledge that I have not breached a public work contract for which the cost exceeds \$25,000,000, within the preceding year, for failing to comply with NRS 338.147 and the requirements of a contract in which I have submitted within 2 hours of the bid opening an Affidavit pertaining to preference eligibility.
8. Upon faxed or mailed receipt of a Notice of Intent to Award the Contract, I will provide the following submittals within seven business days from receipt of the Notice:
  - a) Performance Bond, Labor and Material Payment Bond and a Guaranty Bond, for 100% of the Contract amount as required.
  - b) ~~Certificates of insurance for Commercial General Liability in the amount of \$1,000,000, Automobile Liability in the amount of \$1,000,000, and Workers' Compensation insurance issued by an insurer qualified to underwrite Workers' Compensation insurance in the State of Nevada, as required by law.~~

9. I acknowledge that if I do not provide the above submittals on or before the seventh business day after receipt of the Notice of Intent to Award; or do not keep the bonds or insurance policies in effect, or allow them to lapse during the performance of the Contract; I will pay over to the Owner the amount of **\$1,500** per day as liquidated damages.
10. I confirm this bid is genuine and is not a sham or collusive, or made in the interest of, or on behalf of any person not herein named, nor that the Bidder in any manner sought to secure for themselves an advantage over any bidders.
11. I further propose and agree that if my bid is accepted, I will commence to perform the work called for by the contract documents on the date specified in the Notice to Proceed and I will complete all work within the calendar days **specified in the General Conditions.**
12. I further propose and agree that I will accept as full compensation for the work to be performed the price written in the Bid Schedule below.
13. I have carefully checked the figures below and the Owner will not be responsible for any error or omissions in the preparation or submission of this Bid.
14. I agree no verbal agreement or conversation with an officer, agent or employee of the owner, either before or after the execution of the contract, shall affect or modify any of the terms or obligations of this Bid.
15. I am responsible to ascertain the number of addenda issued, and I hereby acknowledge receipt of the following addenda:

Addendum No. _____ dated, _____	Addendum No. _____ dated, _____
Addendum No. _____ dated, _____	Addendum No. _____ dated, _____
Addendum No. _____ dated, _____	Addendum No. _____ dated, _____
Addendum No. _____ dated, _____	Addendum No. _____ dated, _____
Addendum No. _____ dated, _____	Addendum No. _____ dated, _____

16. I agree to perform all work described in the drawings, specifications, and other documents for the amounts quoted below:

<b>BID SCHEDULE</b>				
ITEM NUMBER	ITEM DESCRIPTION	APPROX. QUANTITY	UNIT	TOTAL
107.01	TRAFFIC CONTROL	60	DAY (F)	
109.01	CONSTRUCTION CONFLICTS AND ADDITIONAL WORK	1	LS	\$100,000.00
109.02	HISTORICAL OWNER CAUSED DELAY ALLOWANCE	5	DAY	\$2,500.00
109.03	ADDITIONAL AMOUNT OVER \$500/DAY AS DETERMINED BY BIDDER	5	DAY	
200.01	MOBILIZATION	1	LS	
202.01	REMOVE PAVEMENT MARKINGS	7,310	SF	
<b>302.01</b>	<b>PULVERIZE ROADWAY</b>	<b>5,015</b>	<b>CY</b>	
302.02	RECONSTRUCTION OF BASE FAILURES	1,000	CY	
<b>402.01</b>	<b>PLANTMIX BITUMINOUS SURFACE</b>	<b>3,350</b>	<b>TON</b>	
502.01	REMOVE AND RECONSTRUCT CUT-OFF WALL	146	LF	
609.01	VERTICALLY ADJUST MANHOLE GRADE RINGS (STORM DRAIN)	1	EA	
613.01	RECONSTRUCT CONCRETE SIDEWALK	490	SF	
613.02	CONCRETE CROSS GUTTER	820	SF	
613.03	CAST-IN-PLACE DETECTABLE WARNING PANEL (5' X 2')	4	EA	
621.01	VERTICALLY ADJUST SURVEY MONUMENT	1	EA	

BID SCHEDULE				
ITEM NUMBER	ITEM DESCRIPTION	APPROX. QUANTITY	UNIT	TOTAL
627.01	REMOVE AND SALVAGE TRAFFIC SIGN	27	EA	
627.02	REMOVE AND RELOCATE TRAFFIC SIGN	1	EA	
627.03	PERMANENT SIGN POST	12	EA	
627.04	PERMANENT SIGN PANEL (R1-1) (48" X 48")	1	EA	
627.05	PERMANENT SIGN PANEL (R2-1) (30" X 36")	1	EA	
627.06	PERMANENT SIGN PANEL (R1-5) (36" X 36")	2	EA	
627.07	PERMANENT SIGN PANEL (R3-7R) (30" X 30")	7	EA	
627.08	PERMANENT SIGN PANEL (RNV2-9) (36" X 30")	1	EA	
627.09	PERMANENT SIGN PANEL (S1-1) (36" X 36")	3	EA	
627.10	PERMANENT SIGN PANEL (W16-7p) (24" X 12")	2	EA	
<b>627.11</b>	<b>PERMANENT SIGN PANEL (W16-9p) (24" X 12")</b>	<b>4</b>	<b>EA</b>	
627.12	PERMANENT SIGN PANEL(W4-4p) (48" X 24")	1	EA	
<b>627.13</b>	<b>PERMANENT SIGN PANEL (TRAIL BLAZER) (12" X 24")</b>	<b>1</b>	<b>EA</b>	
<b>627.14</b>	<b>PERMANENT SIGN PANEL (R1-1) (36" X 36")</b>	<b>1</b>	<b>EA</b>	
<b>627.15</b>	<b>PERMANENT SIGN PANEL (W11-7) (30" X 30")</b>	<b>2</b>	<b>EA</b>	
<b>627.16</b>	<b>PERMANENT SIGN PANEL (W11-2) (30" X 30")</b>	<b>2</b>	<b>EA</b>	
628.01	TYPE 2 FILM (CROSSWALK)	1,500	SF	
628.02	TYPE 2 FILM (STOP BAR)	690	SF	
628.03	TYPE 2 FILM (ARROW LEGEND)	39	EA	
628.04	TYPE 2 FILM (ONLY LEGEND)	3	EA	
<b>628.05</b>	<b>TYPE 2 FILM (BIKE LEGEND)</b>	<b>3</b>	<b>EA</b>	
628.06	TYPE 2 FILM (VARIES SOLID WHITE)	220	SF	
<b>628.07</b>	<b>TYPE 2 FILM (VARIES SOLID YELLOW)</b>	<b>910</b>	<b>SF</b>	
<b>628.08</b>	<b>TYPE 1 FILM (6" SOLID WHITE)</b>	<b>630</b>	<b>SF</b>	
628.09	TYPE 1 FILM (8" SOLID WHITE)	455	SF	
628.10	WHITE POLYUREA PAINT MARKING (6" SOLID)	5,450	LF	
628.11	YELLOW POLYUREA PAINT MARKING (4" SOLID)	2,680	LF	
628.12	TYPE 2 FILM (YIELD MARKINGS)	100	SF	
629.01	VERTICALLY ADJUST WATER VALVE BOX	5	EA	
633.01	PAVEMENT MARKERS (NON-REFLECTIVE)	1,210	EA	
633.02	PAVEMENT MARKERS (REFLECTIVE)	510	EA	
633.03	OVERSIZE PAVEMENT MARKERS (REFLECTIVE)	230	EA	
637.01	DUST CONTROL	60	DAY (F)	
<b>TOTAL BASE BID</b>				<b>\$</b>



17. BUSINESS ENTERPRISE INFORMATION:

The Prime Contractor submitting this Bid is a  MBE  WBE  PBE  SBE  NBE  LBE as defined in the Instructions to Bidders.

18. BUSINESS ETHNICITY INFORMATION:

The Prime Contractor submitting the Bid Ethnicity is  Caucasian (CX)  African American (AA)  Hispanic American (HA)  Asian Pacific American (AX)  Native American (NA)  Other as defined in the Instructions to Bidders.

19. BIDDERS' PREFERENCE Is the Bidder claiming Bidders' Preference?

Yes If yes, the Bidder acknowledges that he/she is required to follow the requirements set forth in the Affidavit (Bid Attachment 3).

No **I do not have a Certificate of Eligibility to receive preference in bidding.**

20.

\_\_\_\_\_  
**LEGAL NAME OF FIRM AS IT WOULD APPEAR IN CONTRACT**

\_\_\_\_\_  
ADDRESS OF FIRM

\_\_\_\_\_  
CITY, STATE, ZIP CODE

\_\_\_\_\_  
TELEPHONE NUMBER

\_\_\_\_\_  
FAX NUMBER

**NEVADA STATE CONTRACTORS' BOARD LICENSE INFORMATION:**

I certify that the license(s) listed below will be the license(s) used to perform the majority of the work on this project.

LICENSE NUMBER: \_\_\_\_\_

LICENSE CLASS: \_\_\_\_\_

LICENSE LIMIT: \_\_\_\_\_

ONE TIME LICENSE LIMIT INCREASE \$ \_\_\_\_\_ IF YES, DATE REQUESTED \_\_\_\_\_

CLARK COUNTY BUSINESS LICENSE NO. \_\_\_\_\_

STATE OF NEVADA BUSINESS LICENSE NO. \_\_\_\_\_

\_\_\_\_\_  
AUTHORIZED REPRESENTATIVE  
(PRINT OR TYPE)

\_\_\_\_\_  
E-MAIL ADDRESS

\_\_\_\_\_  
SIGNATURE OF AUTHORIZED REPRESENTATIVE

\_\_\_\_\_  
TODAY'S DATE

**SECTION 107**

**LEGAL RELATIONS AND RESPONSIBILITY TO THE PUBLIC**

**107.07 TRAFFIC AND ACCESS**

*The following is added to paragraph D:*

There are no bus stops within the project limits. The Contractor shall coordinate with Silver Rider buses regarding bus travel on Thomas Edison Drive during construction.

*Paragraph L is changed to read as follows:*

All costs involved shall be included in the Contractor's bid for traffic control. All barricades, warning signs, lights, temporary signals, and other protective devices must conform with the latest edition of the *Manual for Uniform Traffic Control Devices (MUTCD)*.

*The following is added to this subsection:*

The Contractor shall comply with all requirements contained in Section 624 and 625 of the Standard Specifications and with the supplemental requirements contained herein, except that no direct payment shall be made to the Contractor for signing and/or detours unless otherwise specified herein.

The Contractor, as required by the Engineer, shall provide and station competent flaggers whose sole duties shall consist of directing the movement of public traffic either through or around the work. Traffic work zone technicians on the project must be certified in work zone traffic control at least at the "Technician" level by ATSSA or IMSA.

Where necessary or required for the convenience of the public or individual residents at street crossings, alleys, or at private driveways, the Contractor shall provide suitable temporary bridges over unfilled excavations, except in such cases as the Contractor shall secure permission from the Engineer to omit such temporary bridges. All such bridges shall be maintained in service until after the Contractor has complied with all of the specifications governing the work prior to backfilling.

Temporary bridges for street crossings shall conform to the requirements of the authority having jurisdiction in each case, and shall submit designs for approval by said authority as may be required. Steel plates used on this project must be capable of carrying the heaviest loads utilizing the roadway.

When the Contractor's construction operations encroach upon a sidewalk, walkway, or crosswalk area, the Contractor shall take special precautions to protect the pedestrian's safety including provisions to separate pedestrian traffic from the work area.

Pipe stockpiled along pipeline alignments shall be within 1,000 feet of the Contractor's laying operations. Pipe strung within street right-of-way that is to remain one night or more shall be protected by barricades spaced at intervals not exceeding 50 feet.

Prior to opening the roadway to temporary public traffic at the end of the work day, the contractor shall ensure that no vertical joints or elevation deviations remain in areas open to vehicles. The contractor shall construct all necessary temporary ramps at intersections with driveways and side streets, longitudinal and transverse joints, and roadway conform lines. Temporary ramps shall be constructed of pulverized materials or aggregate base to conform to the elevation of the adjacent existing surface and tapered on a slope of 30:1 horizontal to vertical, or flatter. At driveways, ramps may be tapered to a 12:1 slope, spanning the entire driveway or twenty (20) feet, whichever is wider. The ramps shall be compacted to produce a smooth riding surface. Temporary ramps in conflict with the next sequence of work shall be removed prior to work.

During all phases of work the roadway surface shall be bladed to a smooth surface suitable for vehicular traffic at the end of the each work day. Utility structures that protrude above the graded surface shall have ramps along all edges to allow for the safe passage of vehicles. Placement of barricades, cones, and other traffic warning devices around utility facilities shall not be acceptable as a means to satisfy safe traffic passage.

The following subsections are added:

#### **107.07.01 TRAFFIC CONTROL REGULATIONS**

All traffic and/or traffic control devices under this contract shall be provided, maintained, and/or controlled as specified in this section and in the latest additions of the *MUTCD*.

If the Contractor, at any time, fails to maintain traffic and/or traffic control devices as specified in the above documents or elsewhere by these specifications, the Engineer will immediately notify the Contractor in writing of such non-compliance. If the Contractor fails to remedy unsatisfactory maintenance within two (2) hours after receipt of such notice, the Engineer may immediately proceed to perform such maintenance, and the entire direct cost of this maintenance will be deducted from money due or to become due the Contractor.

If, in the opinion of the Engineer, a condition develops that is dangerous to public safety, such condition may be immediately remedied with whatever means is available to the owner and the cost of this maintenance will be deducted from money due or to become due to the Contractor.

Further, each instance of failure to remedy unsatisfactory maintenance within two (2) hours of notification will result in a deduction of one day's value of the traffic control pay item from the bid amount. One day's value is determined by dividing the bid amount by the number of working or calendar days (whichever is applicable) allowed for the project. This sum is in addition to any direct costs incurred by the Contracting Agency to remedy unsatisfactory maintenance.

#### **107.07.02 TRAFFIC CONTROL REQUIREMENTS**

The Contractor will also, as a minimum, provide the following traffic control measures:

Temporary traffic lanes must be delineated using either paint or traffic tape, excepting that paint may only be used on pavement to be replaced or overlaid as part of this contract.

The tape shall conform to State of Nevada specifications. The temporary pavement striping shall be by pilot line method. The tape shall be 6-inches wide and 4 feet long and spaced every

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RESPONSIBILITY TO THE PUBLIC

40 feet. The color of the tape shall match the color of the line that it replaces. The double yellow line shall have two pieces of tape side by side with a 4-inch space between, and spaced to the increments above.

Painted temporary traffic lines shall be 6-inches wide and shall be continuous or intermittent in accordance with the MUTCD. Existing lines either painted or raised pavement markers, that conflict with required temporary lines shall be effectively removed in such a manner to leave no residue or other trace of the former line that may be misconstrued by a driver to be a traffic line under any condition of daylight, darkness and wetness of pavement.

At the completion of the project, the Contractor shall install additional pilot lines using 4-inch square pieces of tape to show the alignment of any permanent lines that were destroyed during construction. It shall be the responsibility of the Contractor to remark said lines. This 4-inch square tape shall be removed just prior to the installation of the permanent lines.

Type 3 barricades shall be used to protect all approaches to sites of excavation.

Flaggers must be used to assist trucks for safe ingress and egress whenever truck movements may interfere with safe passage through the work zone. In addition, flaggers shall be used whenever the Engineer deems it necessary for safety purposes.

If, in the opinion of the Engineer, a condition develops or exists that is dangerous to the safety of the general public, concrete barrier rails and fencing shall be used around the work site, excavations, trenches for underground utilities, and/or stockpiled materials. The barrier rails and fencing shall be maintained until the Engineer determines that the hazard no longer exists.

Portable concrete barrier rail ("F" rail) shall be used to separate travel lanes from excavations when any excavation:

- a. exceeds one foot in depth,
- b. exceeds one foot in width,
- c. is sloped steeper than 4:1, and
- d. is less than 18 feet from the nearest travel lane.

Fencing shall be used around open trenches exceeding 2' in depth. The fencing may be placed on top of concrete barrier rails. Traffic Control Devices used for delineation shall have Type C steady burn lights. All barricades used for closures shall have Type B lights. Type B lights shall be used with appropriate advance warning signs.

### **107.07.03 TRAFFIC CONTROL AND BARRICADE PLAN**

The Contractor shall submit a written and diagramed 11" x 17" "Traffic Control and Barricade Plan" to the Engineer no later than fourteen (14) calendar days prior to proposed use. Two (2) copies of the plan shall be submitted as a submittal directly to the Project Manager, who will review it for conformance to the contract documents and the MUTCD.

~~No monies for "Traffic Control" shall be deemed to be earned until Contractor obtains approval of a "Traffic Control and Barricade Plan." The total sum subsequently paid for "Traffic Control" shall be the bid amount minus a day's value, as determined in accordance with 107.07.05, for each day after the Notice to Proceed that the Contractor fails to obtain approval for the "Traffic~~

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Control and Barricade Plan." Further, work items requiring traffic control devices to be placed in the traveled way will not be allowed to proceed until the Contractor has an approved "Traffic Control and Barricade Plan" for such work.

Traffic Control Plans shall be submitted on 11" x 17" plans unless required to be full plan size due to the complexity of the plan. All Traffic Control Plans shall be prepared by an ISMA or ATSSA Certified Traffic Control Technician. Each sheet will have the following information:

- Project Name
- Bid Number of the Project
- Name/address of Company that prepared the plan
- Name of the individual that prepared the plan
- The ISMA or ATSSA Certification number of the plan preparer
- The phone number of the Company that prepared the plan
- A 24-hour contact for the Company if different from above
- Whether the setup is day, night, or a 24-hour set up.
- What work will be conducted with the plan
- Date the plan was prepared
- Dates the plan is proposed to be implemented
- Duration of each plan
- A 3" by 5" blank area in the upper right hand corner of each plan sheet
- Roadways shall be oriented with north to the top or left of the plan sheet
- Key to what each symbol used indicates
- If a sand barrel attenuator is used, the barrel layout and fill of each barrel.

Plans shall provide the minimum following items:

- (1) All advance warning signs and arrow boards;
- (2) Method of protecting excavations and work sites;
- (3) Method of barricading at intersections;
- (4) Delineation patterns, lengths, etc., (including laterals);
- (5) All regulatory signs;
- (6) All warning signs within delineation;
- (7) Driveway access plan and business access signage;
- (8) Method for protecting pedestrians and pedestrian access;
- (9) Provisions for emergency vehicle access at all times;
- (10) Lane widths, transitions, and tangent sections.
- (11) Proposed bus stop closures and relocations.
- (12) The existing posted speed on each roadway, and
- (13) Any and all pedestrian pathways.

Temporary Traffic Control Devices shall not be pre-strung or left within the right-of way during non-working hours or when the devices are not being used. Drums shall be used on all tapers and lane shifts.

Contractor is required to provide a minimum of three (3) work day advance written notice to Clark County Traffic Operations for any changes to the traffic or pedestrian signal operations within the accepted traffic control plan. Contractor is responsible for all cost associated with any changes required to the traffic signal for vehicle or pedestrian movements.

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The above described plan must conform to the Special Provisions Section 625 contained herein as well as the MUTCD and ADA requirements for all traffic control methods, devices, and appurtenances.

The Contractor is required to post with the Contracting Agent all information relative to any subcontractor for barricade control, including: Name of firm, address of firm, telephone number of firm, name of responsible agent for the project, and a 24 hour number for emergency response. All traffic control devices must display the 24-hour contact number of the traffic control (sub) contractor on their reverse side, along with an identifying name, initials or logo.

The "Traffic Control and Barricade Plan" must be completed to the Engineer's satisfaction and all traffic control devices installed according to the approved plans before each construction phase begins.

**107.07.04 TRAFFIC CONTROL PLAN FOR HIGHWAY WORK ZONES**

The Contractor may not utilize the "Traffic Control Plans for Highway Work Zones" as shown in the Standard Drawings in lieu of preparing a "Traffic Control and Barricade Plan" per Subsection 107.07.03.

**107.07.05 TRAFFIC CONTROL MEASUREMENT AND PAYMENT**

Measurement for payment for traffic control shall be per contract calendar day for all traffic control required to safely perform the work described in the general and special provisions and the project plans, including punch list and clean up.

The bid amount paid for traffic control shall be full compensation for performing all required control of traffic including barricading, signing, temporary fencing, project signs, temporary lane delineation, arrow boards, portable changeable message signs, removal of conflicting markings, temporary paving to provide a minimum number of paved travel lanes as specified in section 104, detours, driveway access and flag persons, fencing, trench plates, temporary bridges, concrete barrier rail to protect trenches, as specified herein, and as required by the Engineer.

The bid amount payment for traffic control shall include all weekends, holidays and non-working days encountered during the duration of the contract including any days required for completion of corrective punch-list items.

The bid amount will be divided by the number of calendar days in the contract to establish a daily value for Traffic Control. Each progress payment will reflect payment on the bid amount equal to this daily value times the number of days since the previous progress payment. The bid amount for this item shall not be increased. In the event compensable time is added to the contract, additional payment for traffic control shall be included in the payment for Historical Owner Caused Delay Allowance Over \$500 Per Day.

Payment will be made under:

<u>Pay Item</u>	<u>Pay Unit</u>
Traffic Control	Day (F)

### **107.16 CONTRACTOR’S RESPONSIBILITY FOR THE WORK AND MATERIALS**

*The following is added to this subsection:*

The contractor shall maintain on-site, copies of all applicable Material Safety Data Sheets (MSDS) for all materials used on the project.

These MSDS sheets shall be readily available for inspection and review by any individual who may so request.

### **107.18 FURNISHING RIGHT-OF-WAY**

*The following is added to this subsection:*

- (a) Lands or rights-of-way for the work to be constructed under the contract will be provided by the Owner as shown on the Drawings. Nothing contained in the Specifications or Drawings shall be interpreted as giving the Contractor exclusive occupancy of the lands or rights-of-way provided. Any additional lands or rights-of-way required for construction operations shall be provided by the Contractor at his own expense.
- (b) The Contractor shall not enter nor occupy with men, equipment, or materials any lands outside the rights-of-way or easements shown on the Drawings without the written consent of the owner of the property. Evidence of written permission to occupy lands outside those rights-of-way shown shall be presented to the Engineer prior to entry upon said land by the Contractor.

*Add the following subsections:*

### **107.23 PUBLIC RELATIONS AND NOTIFICATION**

The Contractor shall be responsible to keep the residents and business owners within or adjacent to the project informed of the Project. This information shall be as accurate and informative as possible with emphasis placed on timely communication in Spanish and English.

Ten (10) working days after receiving the Notice of Award from Clark County Purchasing and Contracts Division, the Contractor shall prepare a draft public notification for approval by the Engineer that informs all property owners, residents and/or businesses within or adjacent to the project of the project status. The notification letter shall be distributed by the Contractor only after approval of the Engineer.

This notification shall be an introductory letter, which shall contain information such as the scope of the project, an anticipated construction start date and sequence of work, and the Contractors contact information.

Seventy-two (72) hours prior to commencement of construction, the Contractor will notify, in person, all property owners and/or businesses which will be affected by the construction operation and will install “No Parking” signs at no more than 250 foot intervals along each side of the road where vehicle removal is required. Whenever personal notification is not possible, the Contractor, at his expense, shall reproduce and distribute written or printed notification in the

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form of a leaflet, door-hanger, etc., approved by the Engineer, to each residence or business affected.

This notification shall contain information such as the date and time of construction, brief explanation of work, vehicle removal instructions, Contractor's name, business address, and 24-hour telephone number of the Contractor or one of his agents. After notification by the Contractor, the Engineer will make any arrangements necessary to remove vehicles remaining in the roadway at the time of construction.

The cost of this work shall be considered as incidental to other related items of work and no separate payment shall be made therefor. The Contractor shall keep records of the notification process and be able show proof to the Engineer, when required.

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**SECTION 302**

**AGGREGATE BASE COURSES**

**CONSTRUCTION**

**302.03.01 SUBGRADE PREPARATION**

*The following is added to this subsection:*

If subgrade is exposed, the top six inches of subgrade shall be compacted to not less than 90 percent relative compaction, and the moisture content shall be kept at the optimum level until placement of a base course as specified in Subsection 203.03.18.

When caliche is encountered within the excavation limits, the treatment, removal, trimming, and working of that caliche shall be considered as incidental to the excavation work.

The caliche shall be trimmed a minimum of 2 inches below the bottom of the asphalt concrete layer. A minimum 2-inch leveling course of base material must be placed between the caliche and the asphalt.

Proof rolling without testing shall be performed and accepted by the Engineer prior to prime coat and paving.

*The following subsections are added:*

**302.03.07 SCARIFY, GRADE, AND COMPACT EXISTING AGGREGATE BASE**

The existing base course which is exposed after removal of the asphalt pavement shall be supplemented with the recycled mix base to achieve the grade as shown on the plans, or grade for proper drainage and transitions as directed by the Engineer, watered and compacted to 95 percent relative compaction in accordance with subsection 302.03.05, "Compaction," of the Standard Specifications.

Full compensation for the work specified in this subsection shall be considered as included in the price paid for "pulverize roadway" and no other separate payment shall be made therefore.

**302.03.08 RECONSTRUCTION OF BASE FAILURES**

In those areas designated as "base failures" (severe alligator cracking and pothole clusters) by the Engineer, the Contractor will remove the plantmix bituminous surface and all unsuitable material within the subbase to a depth as designated by the Engineer and will reconstruct base failures as specified herein, as shown on the drawings and/or as directed by the Engineer.

The Contractor will repair distressed areas caused by poor base (base failures) having either distorted or broken pavement (severe alligator cracking and pothole clusters). The areas to be repaired shall be sawcut or spadecut in a rectangular shape and to a minimum of one foot outside the area of repair. The edges of the sawcut or spadecut shall be vertical to the surrounding area being repaired.

## SECTION 302 – AGGREGATE BASE COURSES

All damaged plantmix bituminous surface, unsuitable material, and excessive moisture shall be removed to the depth specified by the Engineer, but not exceeding three (3) feet below the surface of the plantmix bituminous surface unless otherwise determined by the Engineer to be necessary. Type II aggregate base with the correct moisture content shall be placed and compacted in lifts not exceeding six inches. If the Contractor uncovers suspected soft and/or deleterious soil conditions, the Engineer shall be notified to visually inspect the uncovered soil surfaces and determine what soil treatments and mitigation are necessary.

All base repairs shall be done prior to reconstructing (pulverizing) and/or overlaying of existing asphalt concrete pavement unless otherwise approved by the Engineer. New plantmix bituminous surface shall be placed over the repaired base, while road paving is done, without leaving any patch marks.

In situations where base repairs are to be done after pavement is replaced and/or overlaid, a light layer of tack coat shall be evenly applied to all exposed cut pavement edges, and a plantmix bituminous surface patch shall be placed to the thickness as shown on the drawings and/or as required by the Engineer.

The Contractor shall be responsible for the hauling and disposing of all materials removed from the repaired areas.

The accepted quantity of plantmix bituminous surface placed over the repaired base shall be paid in accordance with Subsection 402.05.01.

Geotextile soil stabilization fabric may be placed over soft subgrade prior to placement of Type II aggregate base in areas excavated for base repair. The application of geotextile fabric will reduce the amount of Type II aggregate base needed for base repair as shown on the drawings and/or as directed by the Engineer. The physical property of the geotextile fabric can be found in the Geotextile Specification for Highway Applications, AASHTO Designation: M 288.

Geotextile fabric shall be accompanied with a certificate of compliance as required by Section 106.05 of the Standard Specifications. The Contractor shall provide the Engineer with qualification statements of the supervising personnel to be on the job.

The fabric shall be cut, stretched, aligned and placed with no wrinkles that overlap. The condition of the fabric shall be observed by qualified personnel and inspected by the Engineer to determine that no holes or rips exist in the fabric before covering with Type II aggregate base. All such occurrences should be repaired by placing a new layer of fabric extending beyond the defect in all directions at a minimum distance of 2 feet or at a distance equal to the minimum overlap required for adjacent rolls.

The first layer of aggregate should be graded down from the previously placed fill to a thickness determined by the engineer. At no time should equipment be allowed on the fabric with less than 6 inches of aggregate between the wheels and the fabric. Construction vehicles should be limited in size and weight such that rutting in the initial lift is less than 3 inches. If rut depths exceed 3 inches, then the size and weight of the equipment shall be reduced.

The first lift should be compacted by "Tracking" with a dozer or by other method approved by the engineer and then subsequent layers may be compacted to the required density. For soft

soils the design density of the first layer should be 5% less than for the remaining layers to minimize disturbance of the subgrade. Use of a vibratory roller should be closely monitored in applications where the existing subgrade consists of saturated, loose cohesionless soils. Liquefaction of these soils due to excess pore water pressure can lead to bearing capacity failure of the subgrade. Turning of construction equipment should not be permitted on the first layer of aggregate placed upon the fabric.

Any ruts that form during spreading or compacting should be filled with additional aggregate to maintain an adequate depth of cover over the geotextile. In no case should ruts be simply bladed down. All remaining lifts of aggregate base shall be placed and compacted in lifts not exceeding 6 inches.

**302.03.09 PULVERIZE, MIX, COMPACT AND SUPPLEMENT AGGREGATE BASE**

Existing asphalt concrete pavement and portion of base material, where required to be restored, will be pulverized to a depth as shown on the drawings and/or as required by the Engineer, mixed and compacted and, thereafter, Type II aggregate base will be supplemented, if necessary, in accordance with the requirements specified hereinafter and in conformity with the line, thickness and cross sections shown on the drawings or established by the Engineer. The cost of any supplemental Type II aggregate base shall be included in the unit cost to pulverize and pave and no additional payment will be made therefore.

If the contractor believes that the existing conditions of portions of street sections are such that removing and replacing pavement or other method can achieve the proposed street section shown in the drawings more efficiently than pulverizing and paving, the contractor may recommend the alternative method to the Engineer. If the Contractor and Engineer agree to the alternative method, all costs involved shall be included in the related line items for pulverize roadway and plantmix bituminous material, and no additional costs will be paid.

- (a) Gradation Requirements. The existing bituminous surface and gravel base shall be pulverized to the extent it will conform to the Type II criteria or the following sieve sizes:

Sieve Sizes	Percentage by Weight Passing Sieve
3-inch	100
2-inch	90-100
# 4	25-65
# 200	2-15

The percentage of moisture shall not exceed optimum.

Contractor shall log the depths of pulverization (or limits of removal and replacement of pavement) and provide the information upon request.

Any section of street with a pavement thickness greater than typical for the majority of streets within the particular residential area is more likely to need additional Type II aggregate base (or recycled mix base from streets that have thinner asphalt resulting in a lower percentage of asphalt in the recycled mix base).

## SECTION 302 – AGGREGATE BASE COURSES

- (b) Weather Limitations. The pulverization and blending operation shall not be allowed to proceed when the atmospheric temperature is below thirty-five (35) degrees Fahrenheit.
- (c) Preparation of Road Bed. The required depth of existing bituminous surface and gravel base shall be pulverized, blended and compacted to ninety-five (95) percent relative maximum density. The Contractor is responsible to ensure that the compacted base shall remain moist until the required Type II aggregate base is supplemented; however, in no case shall the water content exceed optimum. No more of the existing roadway surface shall be pulverized in any working day than can be supplemented with Type II aggregate base in that working day unless otherwise determined by the Engineer. All equipment and tools for the pulverizing operation shall be subject to the approval of the Engineer. The Engineer may, at his discretion, request demonstrations of all equipment, tools and operations prior to and/or during pulverization. The Contractor will not be paid directly for such demonstrations, but will be considered as incidental to the pulverizing operations.

Any excess pulverized material, as determined by the Contractor and as approved by the Engineer to be surplus due to the requirements of the roadway crown as detailed in the plans, shall be temporarily deposited/stockpiled in area(s) located close to this project and arranged by the Contractor, until this project is substantially completed, so that the pulverized material is available for possible reuse on this project. The excess pulverized material may be reused in preparation of roadbeds, base repair and/or shoulder dressing areas upon approval of the Engineer. The excess pulverized material shall be removed and properly disposed of, reused, stockpiled or deposited on an offsite location with written permission of the property owner, and as approved by the Engineer.

**The contractor shall be responsible for keeping the pulverized surface relatively smooth, as determined by the engineer, during exposure to temporary traffic. This work shall include keeping the pulverized surface free of excessive rutting, raveling or pot holes. Before starting daily pulverized road bed activities, contractor shall sweep the pulverized roadbed area constructed the previous day to remove loose material. Prior to paving, the pulverized material shall be tested for compaction of ninety-five (95) percent relative maximum density. The finished road bed surface shall not vary by more than 0.10' from the grades shown on the plans.**

Full compensation for the work specified in this subsection shall be considered as included in the price paid for pulverize roadway and no other separate payment shall be made therefore.

### METHOD OF MEASUREMENT

#### 302.04.01 MEASUREMENT

*The following are added to this subsection:*

The quantity of pulverize roadway to be measured for payment will be the number of cubic yards. This include areas identified in the plans as limits of pulverize and pave; limits of additional pavement; and limits of existing pavement to be removed. Any aggregate base needed for the work is incidental to "pulverize roadway" regardless of the method of construction used to meet the required pavement sections.

SECTION 302 – AGGREGATE BASE COURSES

The quantity of reconstruction of base failure to be measured for payment will be the number of cubic yards repaired as required.

**BASIS OF PAYMENT**

**302.05.01 PAYMENT**

*The first paragraph of this subsection is changed to read as follows:*

The contract unit price paid for Pulverize Roadway shall be full compensation for sawcutting, pulverizing and blending existing surface and base, excavation, any supplemental Type II aggregate base, grading, compacting and maintaining the base, any removal of asphalt pavement and scarification of exposed base course, hauling, placement and disposal of excess pulverized material, and for all labor, tools, equipment, and incidentals necessary to complete the work as shown on the drawings and described in the special provisions, as specified herein and as directed by the Engineer.

The contract unit price paid for Reconstruction of Base Failures shall be full compensation for excavation in excess of the pulverization depth specified for the area, and all processing, incidentals and working of the reconstruction process beyond that already paid for in other items of work. It also includes the full compensation for all equipment, labor and materials including fabric storage and protection prior to placing of Geotextile Soil Stabilization Fabric and all incidentals necessary to complete the work as specified herein and to the satisfaction of the Engineer.

Payment will be made under:

<u>Pay Item</u>	<u>Pay Unit</u>
Pulverize Roadway	Cubic Yard
Reconstruction of Base Failures	Cubic Yard

SECTION 302 – AGGREGATE BASE COURSES

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**SECTION 627**

**PERMANENT SIGNS**

**DESCRIPTION**

**627.01.02 SHOP DRAWINGS**

*Paragraph C of this subsection is changed to read as follows:*

Shop drawings for other than overhead signs will not be required; however, all signs shall conform to the plans and the M.U.T.C.D. For signs that are shown on the plans and are not included in the M.U.T.C.D., special sign details shall be shown on the Contract Drawings. The special details shall include such information as letter heights, spacing between letters and words, borders, sign radii, etc. Standard sign types shall be called out by function code (type) and size on the Contract Drawings.

**METHOD OF MEASUREMENT**

**627.04.01 MEASUREMENT**

*This subsection is changed to read as follows:*

Measurement for payment for Remove and Salvage Traffic Sign, Remove and Relocate Traffic Sign, Permanent Sign Post, and Permanent Sign Panel (Type) will be per each, complete and in place, as shown on the plans.

**BASIS OF PAYMENT**

**627.05.01 PAYMENT**

*This subsection is changed to read as follows:*

The contract unit price paid for Remove and Salvage Traffic Sign shall be full compensation for doing all work including, but not limited to, removal of sign post regardless of quantity of sign panels on post, salvage of sign panels and post to the Clark County Traffic Operations Shop, 5821 East Flamingo Road, removal and proper disposal of all items not salvaged, and for all labor, tools, incidentals, and equipment necessary to complete the work specified herein, and as directed by the Engineer.

The contract unit price paid for Remove and Relocate Traffic Sign shall be full compensation for furnishing and installing all materials, hardware, labor, tools supplied, and incidentals for removal of sign post regardless of quantity of sign panels on post, removal and proper disposal of all items not re-used, erecting permanent signs, complete in place, as shown on the plans and as shown herein specified, including excavation, backfill, foundations, and hardware.

The contract unit price paid for Permanent Sign Post shall be full compensation for furnishing and installing all materials, labor, tools supplied and incidentals for doing all the work involved in furnishing and erecting permanent signs, complete in place, as shown on the plans and as shown herein specified, including excavation, backfill, foundations, and hardware.

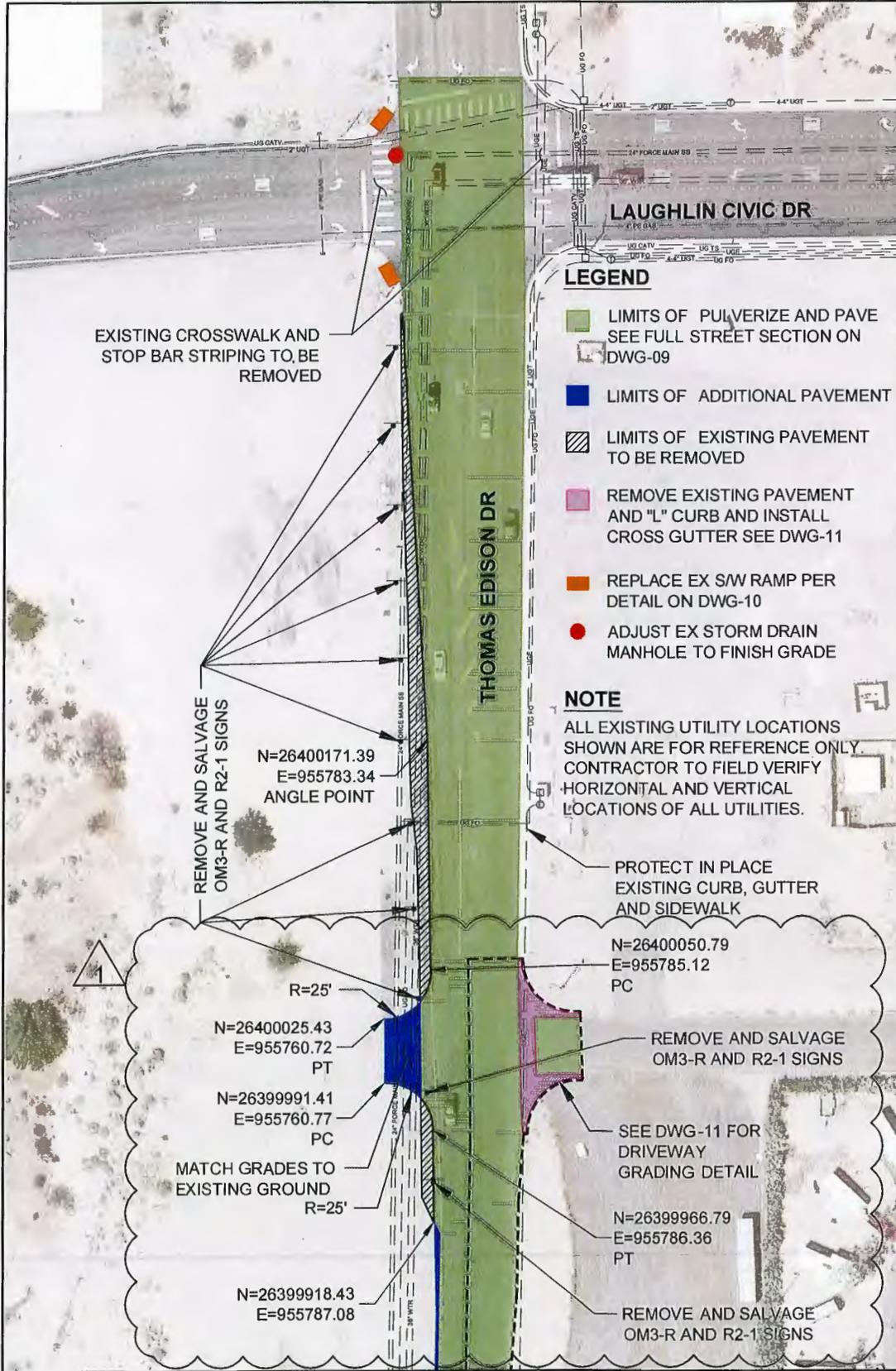
SECTION 627 – PERMANENT SIGNS

The contract unit price paid for Permanent Sign Panel (R1-1) (48"x48"), Permanent Sign Panel (R2-1) (30"x36"), Permanent Sign Panel (R1-5) (36"x36"), Permanent Sign Panel (R3-7R) (30"x30"), Permanent Sign Panel (R3-7R) (36"x30"), Permanent Sign Panel (S1-1) (36"x36"), Permanent Sign Panel (W16-7p) (24"x12"), Permanent Sign Panel (W16-9p) (24"x12"), Permanent Sign Panel (W4-4p) (48"x24"), **Permanent Sign Panel (Trail Blazer) (12"x24")**, **Permanent Sign Panel (R1-1) (36"x36")**, **Permanent Sign Panel (W11-7) (30"x30")**, **Permanent Sign Panel (W11-2) (30"x30")**, shall be full compensation for furnishing and installing all materials, hardware, labor, tools supplied and incidentals for doing all the work involved in furnishing and erecting permanent signs, complete in place, as shown on the plans and as shown herein specified, including, supports and brackets for mounting on either steel sign posts, streetlights and traffic signal poles.

All payments will be made in accordance with Subsection 109.02, "Scope of Payment."

Payment will be made under:

<u>Pay Item</u>	<u>Pay Unit</u>
Remove and Salvage Traffic Sign	Each
Remove and Relocate Traffic Sign	Each
Permanent Sign Post	Each
Permanent Sign Panel (R1-1) (48"x48")	Each
Permanent Sign Panel (R2-1) (30"x36")	Each
Permanent Sign Panel (R1-5) (36"x36")	Each
Permanent Sign Panel (R3-7R) (30"x30")	Each
Permanent Sign Panel(RNV2-9) (36"x30")	Each
Permanent Sign Panel (S1-1) (36"x36")	Each
Permanent Sign Panel (W16-7p) (24"x12")	Each
Permanent Sign Panel (W16-9p) (24"x12")	Each
Permanent Sign Panel (W4-4p) (48"x24")	Each
<b>Permanent Sign Panel (Trail Blazer) (12"x24")</b>	<b>Each</b>
<b>Permanent Sign Panel (R1-1) (36"x36")</b>	<b>Each</b>
<b>Permanent Sign Panel (W11-7) (30"x30")</b>	<b>Each</b>
<b>Permanent Sign Panel (W11-2) (30"x30")</b>	<b>Each</b>



EXISTING CROSSWALK AND STOP BAR STRIPING TO BE REMOVED

REMOVE AND SALVAGE OM3-R AND R2-1 SIGNS

N=26400171.39  
E=955783.34  
ANGLE POINT

N=26400025.43  
E=955760.72  
PT

N=26399991.41  
E=955760.77  
PC

MATCH GRADES TO EXISTING GROUND  
R=25'

N=26399918.43  
E=955787.08

**LEGEND**

- LIMITS OF PULVERIZE AND PAVE SEE FULL STREET SECTION ON DWG-09
- LIMITS OF ADDITIONAL PAVEMENT
- LIMITS OF EXISTING PAVEMENT TO BE REMOVED
- REMOVE EXISTING PAVEMENT AND "L" CURB AND INSTALL CROSS GUTTER SEE DWG-11
- REPLACE EX SW RAMP PER DETAIL ON DWG-10
- ADJUST EX STORM DRAIN MANHOLE TO FINISH GRADE

**NOTE**

ALL EXISTING UTILITY LOCATIONS SHOWN ARE FOR REFERENCE ONLY. CONTRACTOR TO FIELD VERIFY HORIZONTAL AND VERTICAL LOCATIONS OF ALL UTILITIES.

PROTECT IN PLACE EXISTING CURB, GUTTER AND SIDEWALK

N=26400050.79  
E=955785.12  
PC

REMOVE AND SALVAGE OM3-R AND R2-1 SIGNS

SEE DWG-11 FOR DRIVEWAY GRADING DETAIL

N=26399966.79  
E=955786.36  
PT

REMOVE AND SALVAGE OM3-R AND R2-1 SIGNS

**REVISION**

04/23/2014 - PER ADDENDUM #1  
ADDED DRIVEWAY ON WEST SIDE OF THOMAS EDISON DR

**PAVEMENT REPLACEMENT PLAN**

**PULVERIZE AND PAVE - THOMAS EDISON DRIVE**

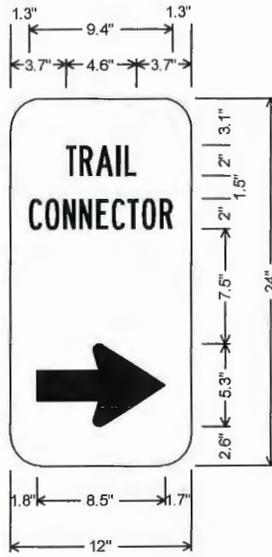
MATCHLINE - SEE DRAWING 05

1



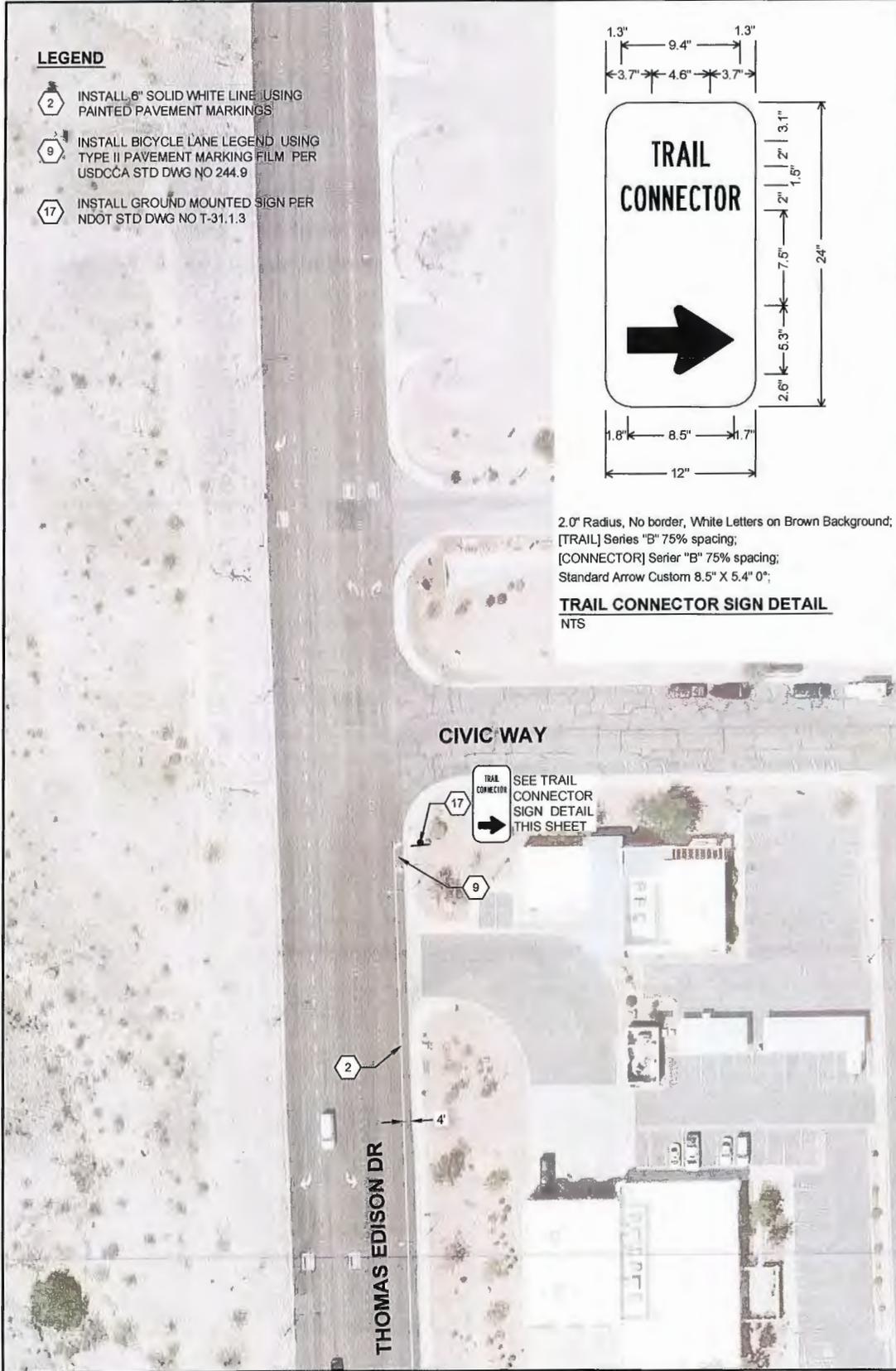
**LEGEND**

- 2 INSTALL 6" SOLID WHITE LINE USING PAINTED PAVEMENT MARKINGS
- 9 INSTALL BICYCLE LANE LEGEND USING TYPE II PAVEMENT MARKING FILM PER USDCCA STD DWG NO 244.9
- 17 INSTALL GROUND MOUNTED SIGN PER NDOT STD DWG NO T-31.1.3



2.0' Radius, No border, White Letters on Brown Background;  
 [TRAIL] Series "B" 75% spacing;  
 [CONNECTOR] Series "B" 75% spacing;  
 Standard Arrow Custom 8.5" X 5.4" 0";

**TRAIL CONNECTOR SIGN DETAIL**  
 NTS



MATCHLINE - SEE DRAWING 14.1

**REVISION**

04/23/2014 - PER ADDENDUM #1  
 1 ADDED SHEET

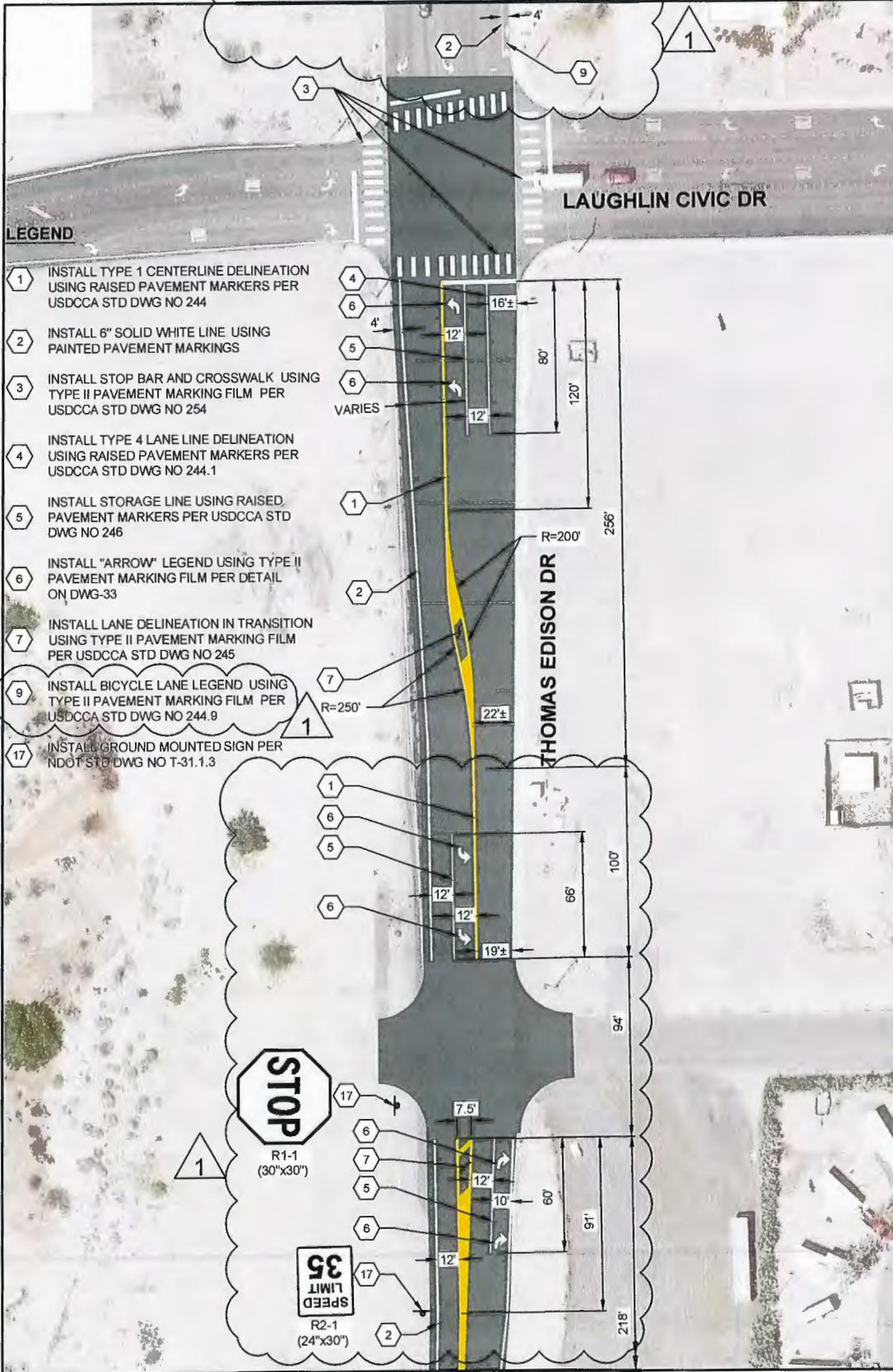
**SIGNING AND STRIPING PLAN**

**PULVERIZE AND PAVE -  
 THOMAS EDISON DRIVE**

DWG - 14



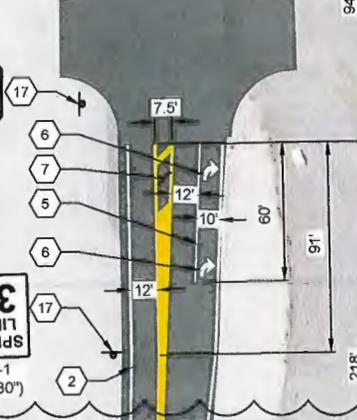
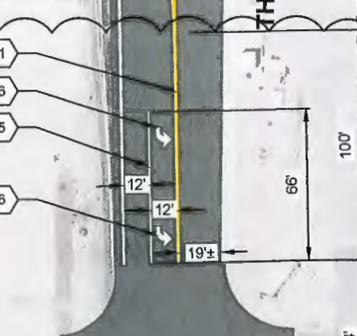
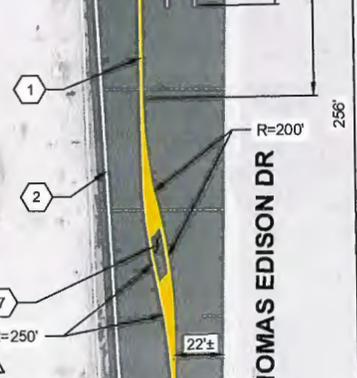
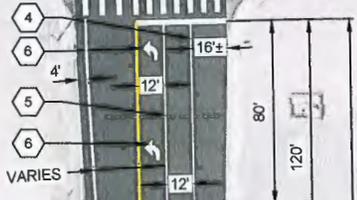
MATCHLINE - SEE DRAWING 14



MATCHLINE - SEE DRAWING 15

**LEGEND**

- 1 INSTALL TYPE 1 CENTERLINE DELINEATION USING RAISED PAVEMENT MARKERS PER USDCCA STD DWG NO 244
- 2 INSTALL 6" SOLID WHITE LINE USING PAINTED PAVEMENT MARKINGS
- 3 INSTALL STOP BAR AND CROSSWALK USING TYPE II PAVEMENT MARKING FILM PER USDCCA STD DWG NO 254
- 4 INSTALL TYPE 4 LANE LINE DELINEATION USING RAISED PAVEMENT MARKERS PER USDCCA STD DWG NO 244.1
- 5 INSTALL STORAGE LINE USING RAISED PAVEMENT MARKERS PER USDCCA STD DWG NO 246
- 6 INSTALL "ARROW" LEGEND USING TYPE II PAVEMENT MARKING FILM PER DETAIL ON DWG-33
- 7 INSTALL LANE DELINEATION IN TRANSITION USING TYPE II PAVEMENT MARKING FILM PER USDCCA STD DWG NO 245
- 9 INSTALL BICYCLE LANE LEGEND USING TYPE II PAVEMENT MARKING FILM PER USDCCA STD DWG NO 244.9
- 17 INSTALL GROUND MOUNTED SIGN PER NDOT STD DWG NO T-31.1.3



**SIGNING AND STRIPING PLAN**

REVISION  
 04/23/2014 - PER ADDENDUM #1  
 ADDED DRIVEWAY, UPDATED STRIPING,  
 SIGNAGE, LEGEND AND DRAWING NUMBER

**PULVERIZE AND PAVE - THOMAS EDISON DRIVE**

1

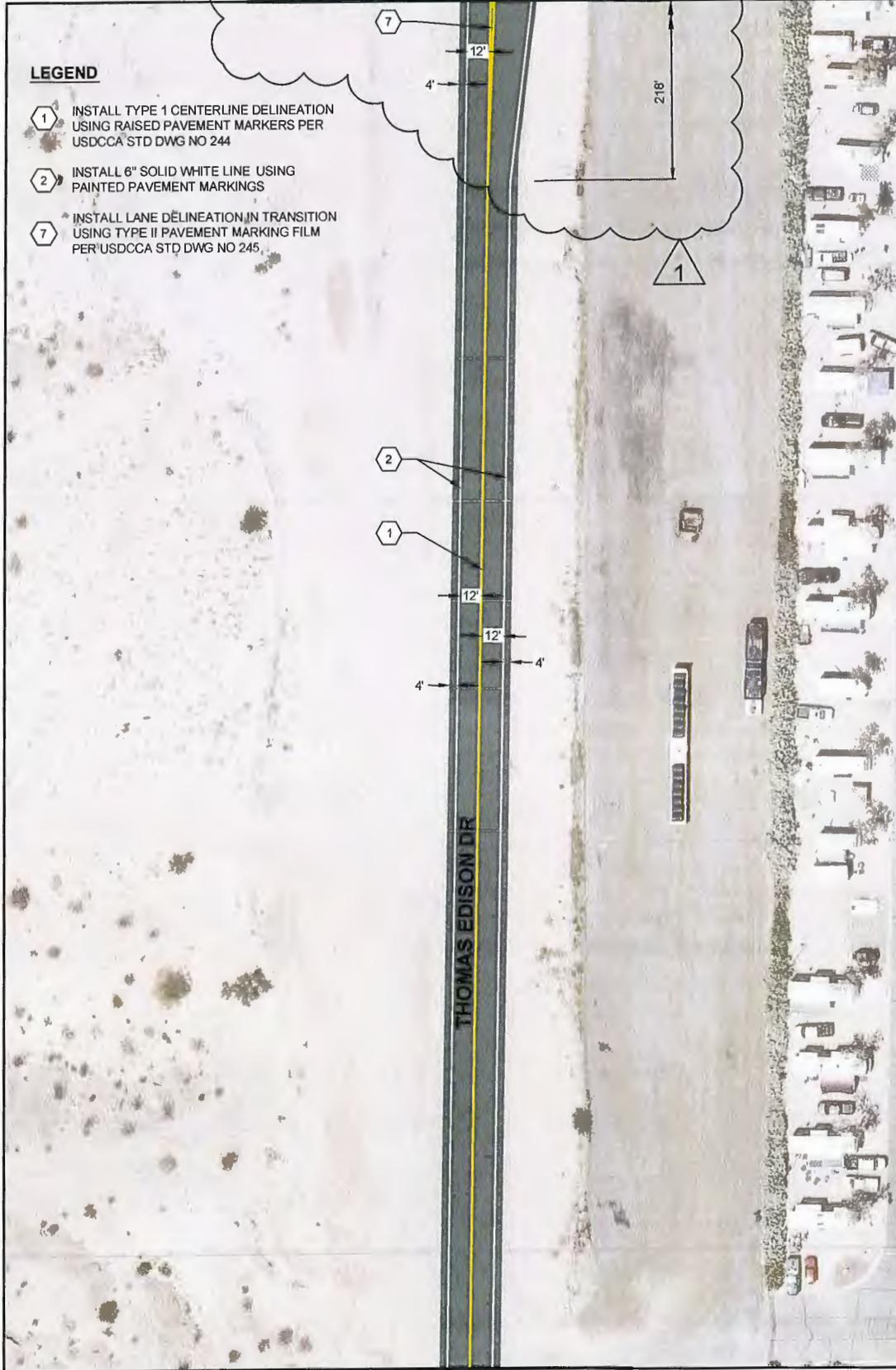
DWG - 14.1



MATCHLINE - SEE DRAWING 14.1

**LEGEND**

- 1 INSTALL TYPE 1 CENTERLINE DELINEATION USING RAISED PAVEMENT MARKERS PER USDCCA STD DWG NO 244
- 2 INSTALL 6" SOLID WHITE LINE USING PAINTED PAVEMENT MARKINGS
- 7 INSTALL LANE DELINEATION IN TRANSITION USING TYPE II PAVEMENT MARKING FILM PER USDCCA STD DWG NO 245



MATCHLINE - SEE DRAWING 16

**REVISION**

- 04/23/2014 - PER ADDENDUM #1  
UPDATED DIMENSION AND DRAWING  
REFERENCE
- 1

**SIGNING AND STRIPING PLAN**

**PULVERIZE AND PAVE -  
THOMAS EDISON DRIVE**

DWG - 15

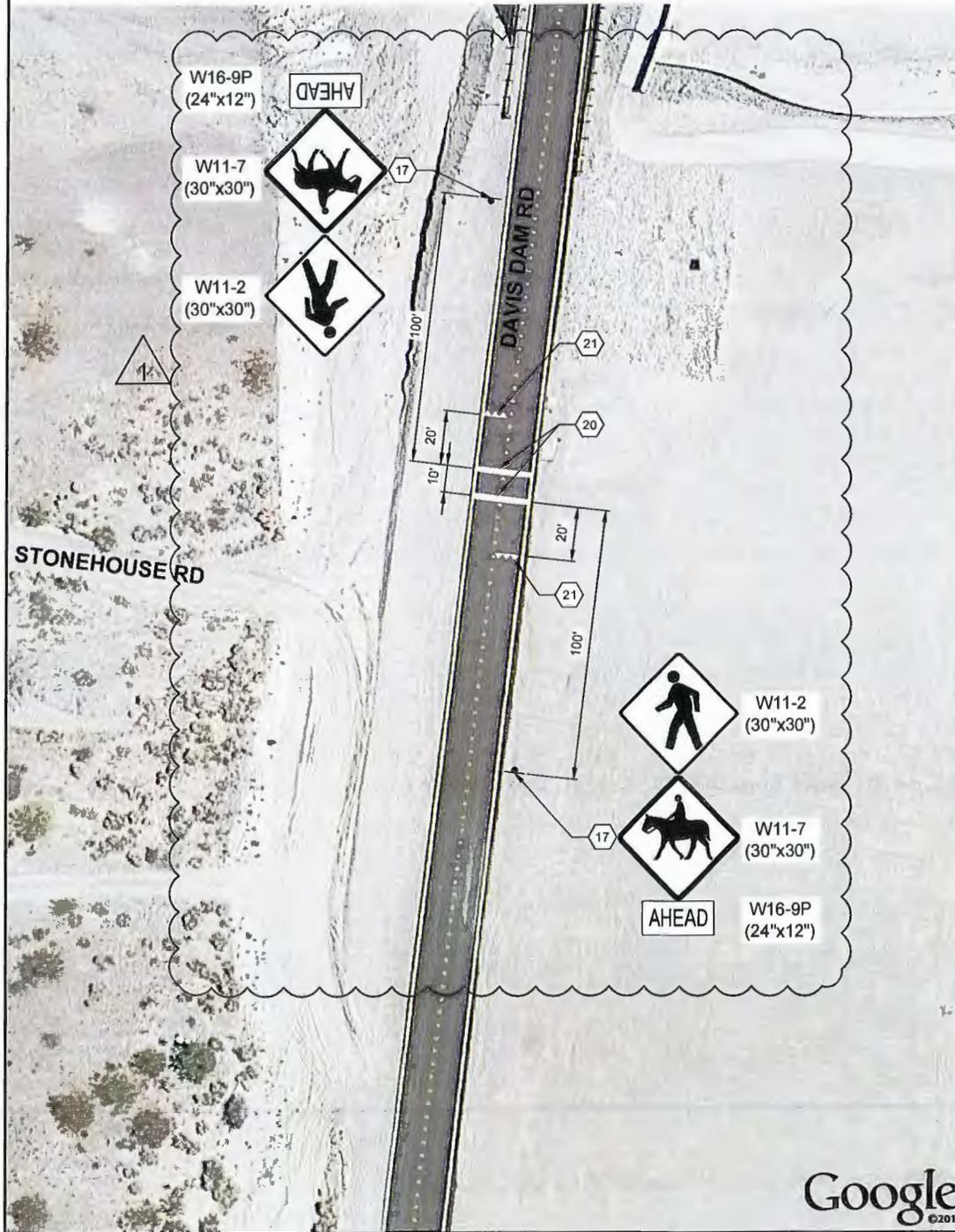


**LEGEND**

-  INSTALL GROUND MOUNTED SIGN PER NDOT STD DWG NO T-31.1.3 
-  INSTALL CROSSWALK USING TYPE II PAVEMENT MARKING FILM PER USCCA STD DWG NO 254.1.S1
-  INSTALL YIELD MARKINGS USING TYPE II PAVEMENT MARKING FILM

**NOTE**

FIELD VERIFY LOCATION OF THE CROSS WALK.



**REVISION**

- 04/23/2014 - PER ADDENDUM #1
-  UPDATED STRIPING, LEGEND AND ADD SIGNAGE

**STRIPING PLAN**

**DAVIS DAM ROAD MODIFICATIONS**

DWG - 38



**LEGEND**

-  INSTALL CROSSWALK USING TYPE II PAVEMENT MARKING FILM PER USDCCA STD DWG NO 254.1.S1
-  INSTALL YIELD MARKINGS USING TYPE II PAVEMENT MARKING FILM

**NOTE**

FIELD VERIFY LOCATION OF THE CROSS WALK.



**STRIPING PLAN**

REVISION  
04/23/2014 - PER ADDENDUM #1  
 SHOW EXISTING SIGN

**DAVIS DAM ROAD MODIFICATIONS**