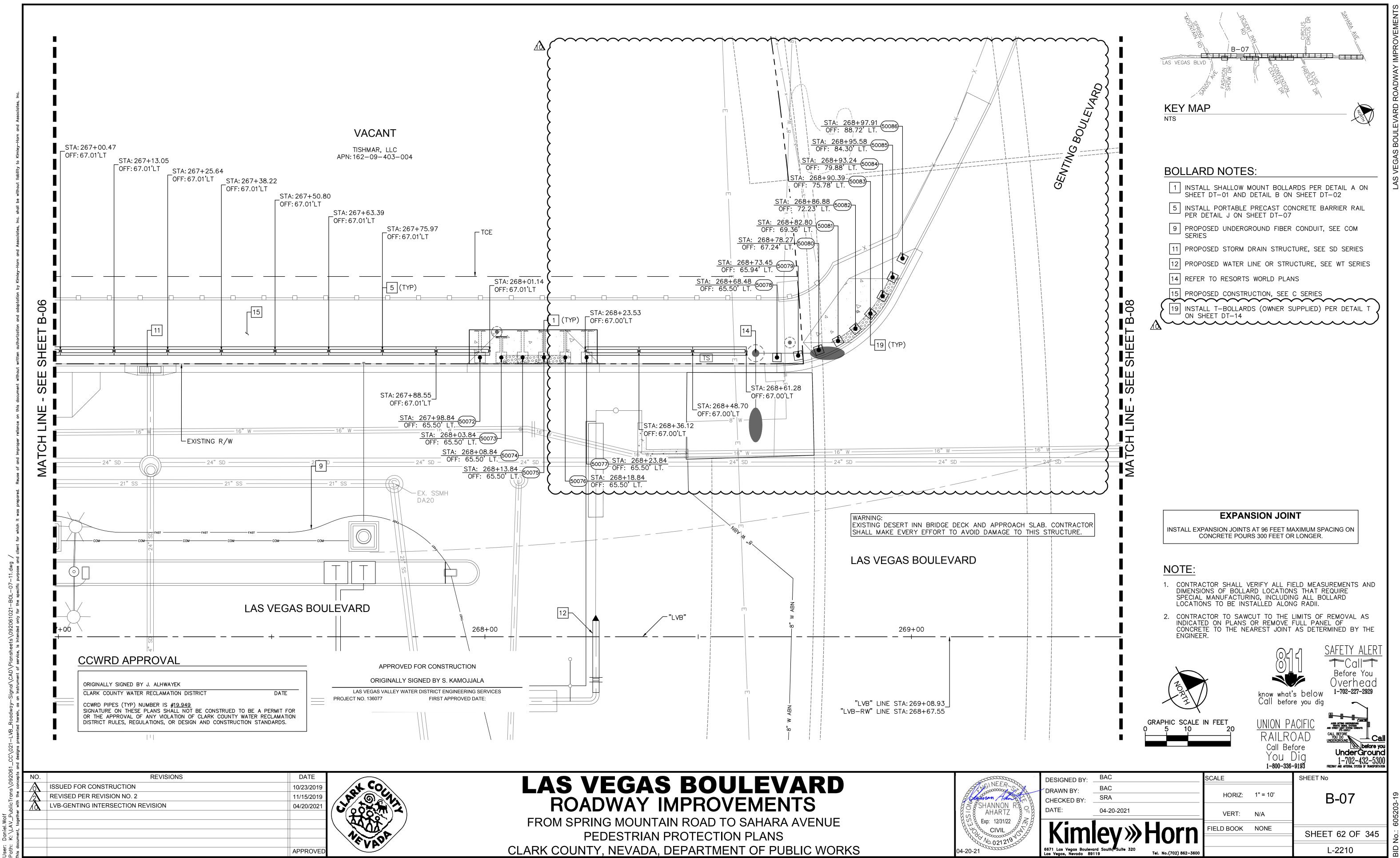
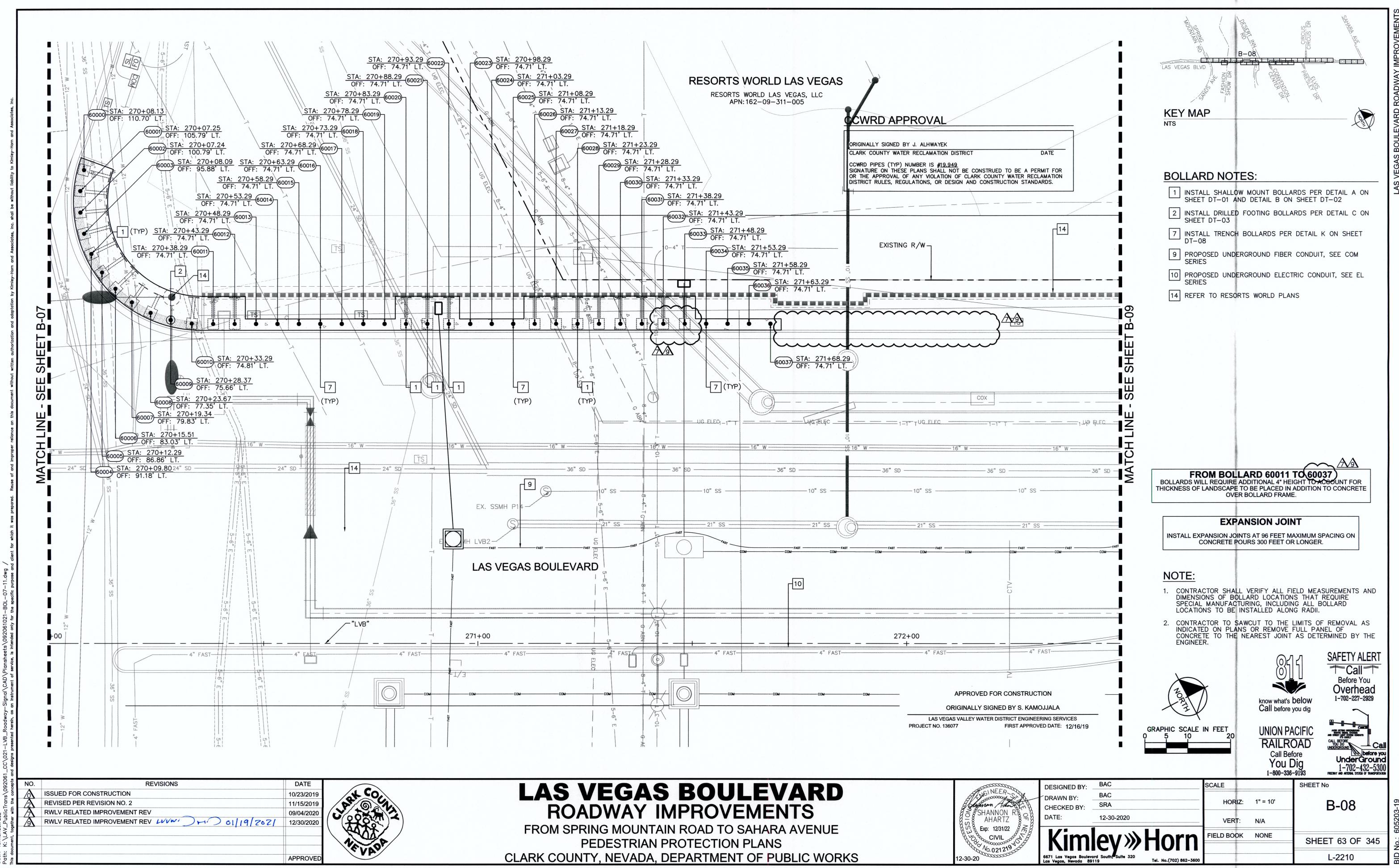
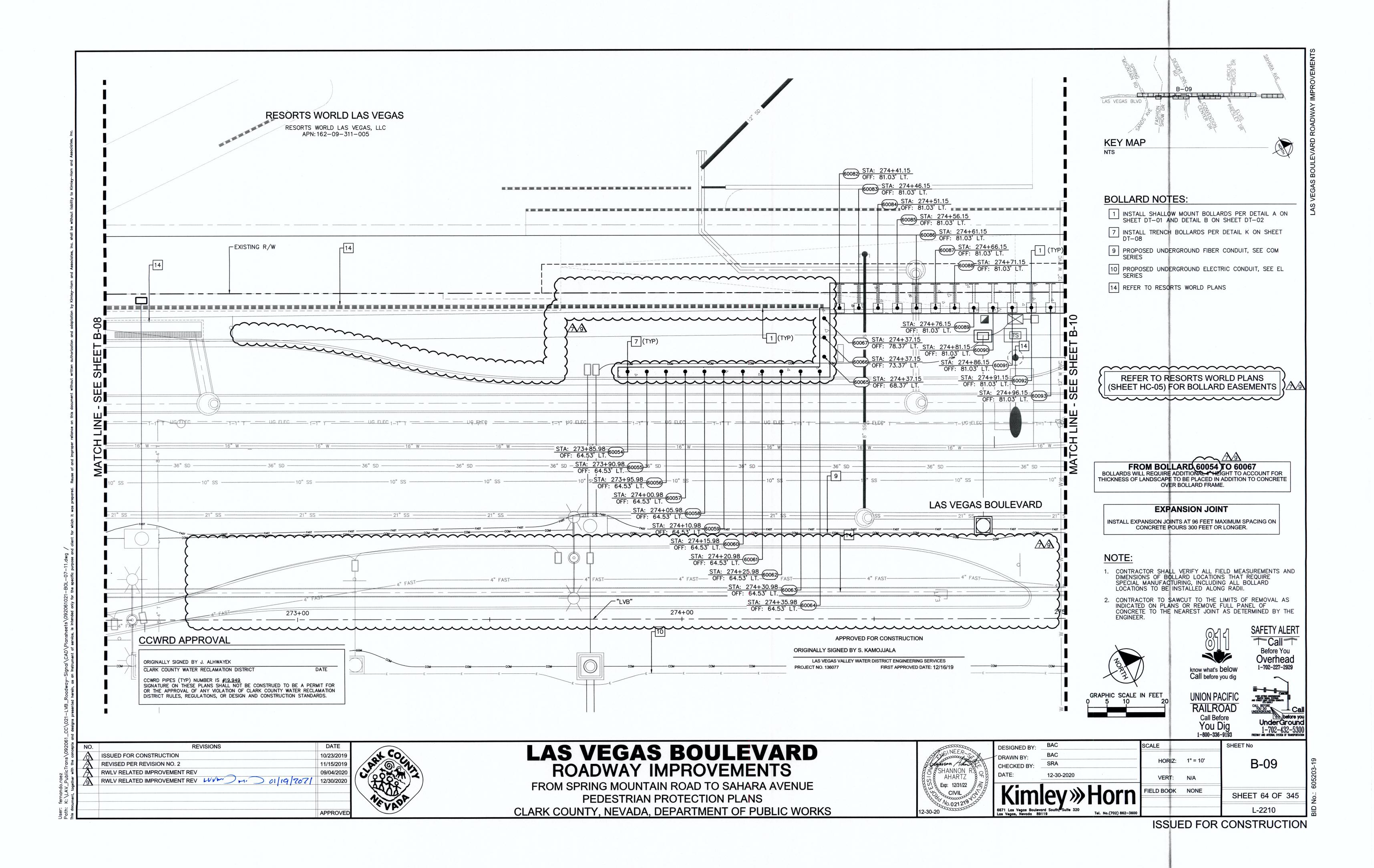


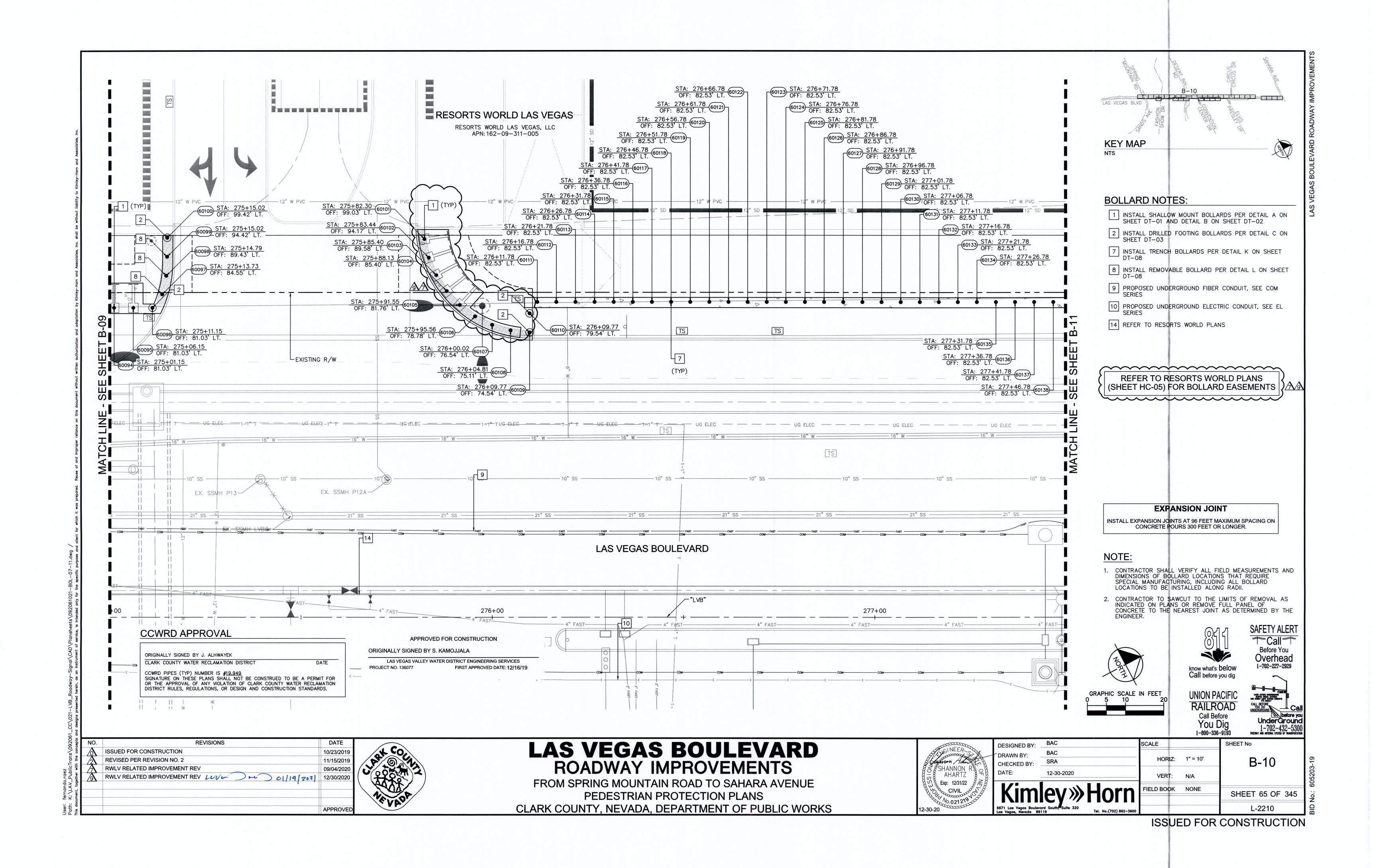
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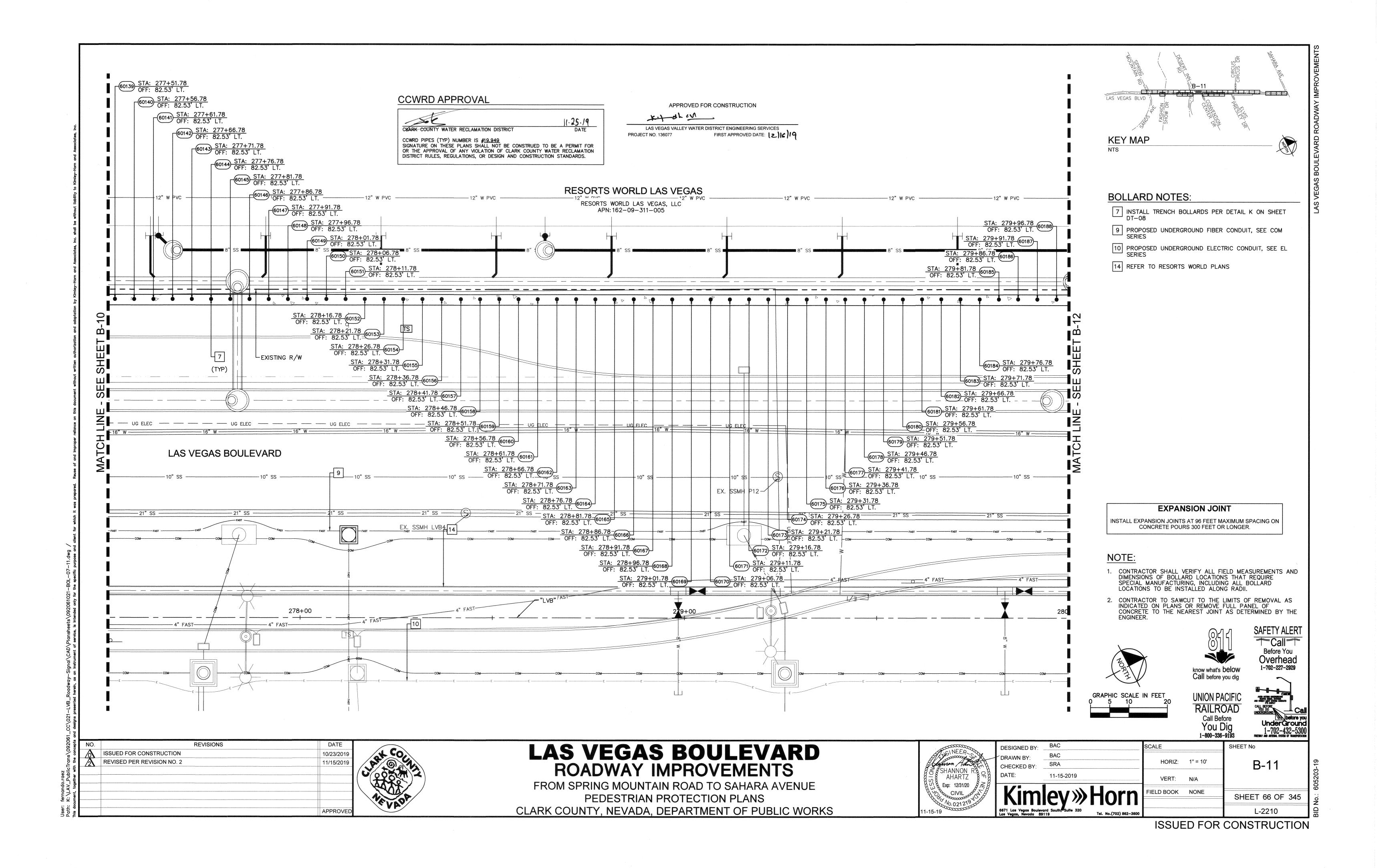


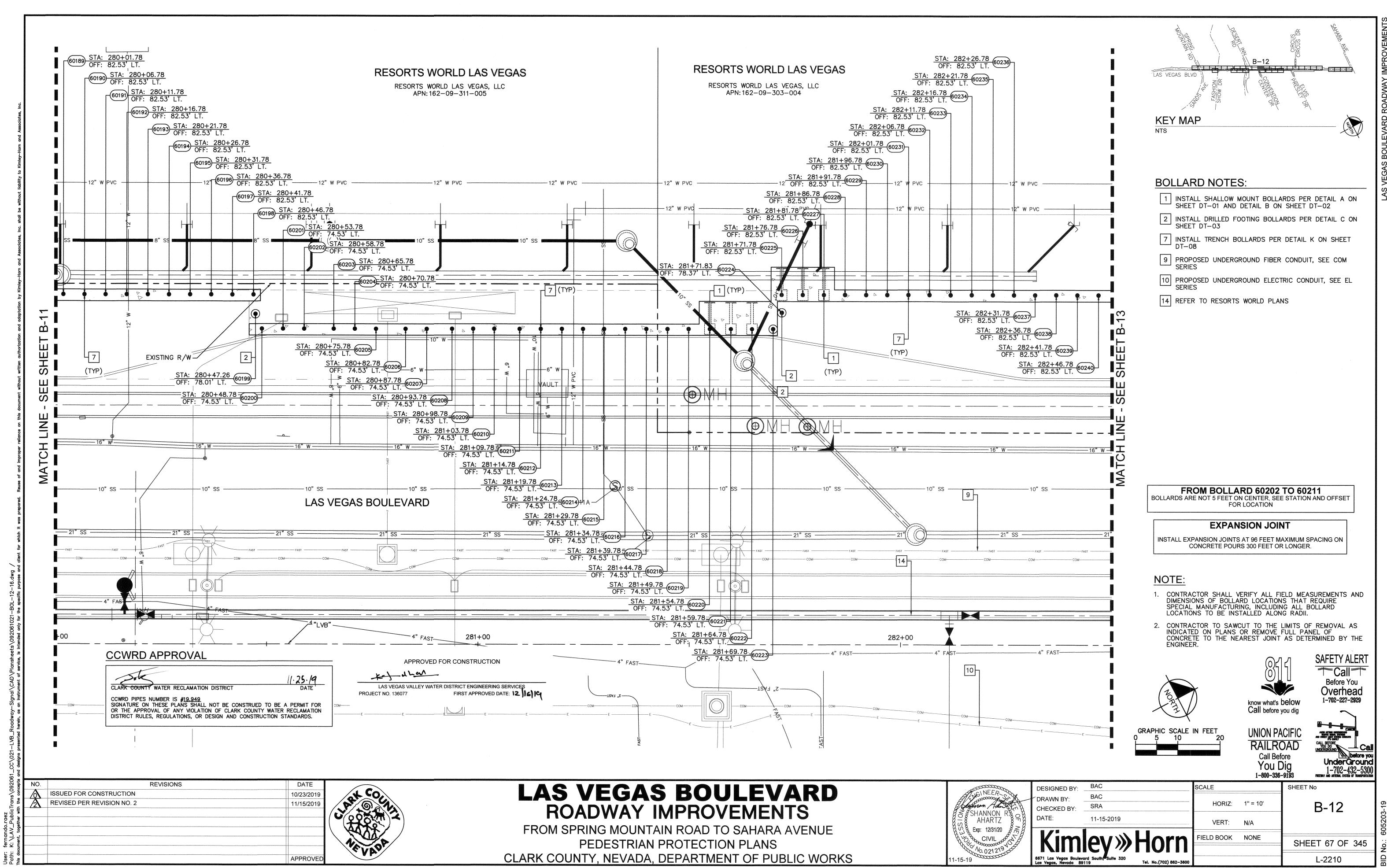


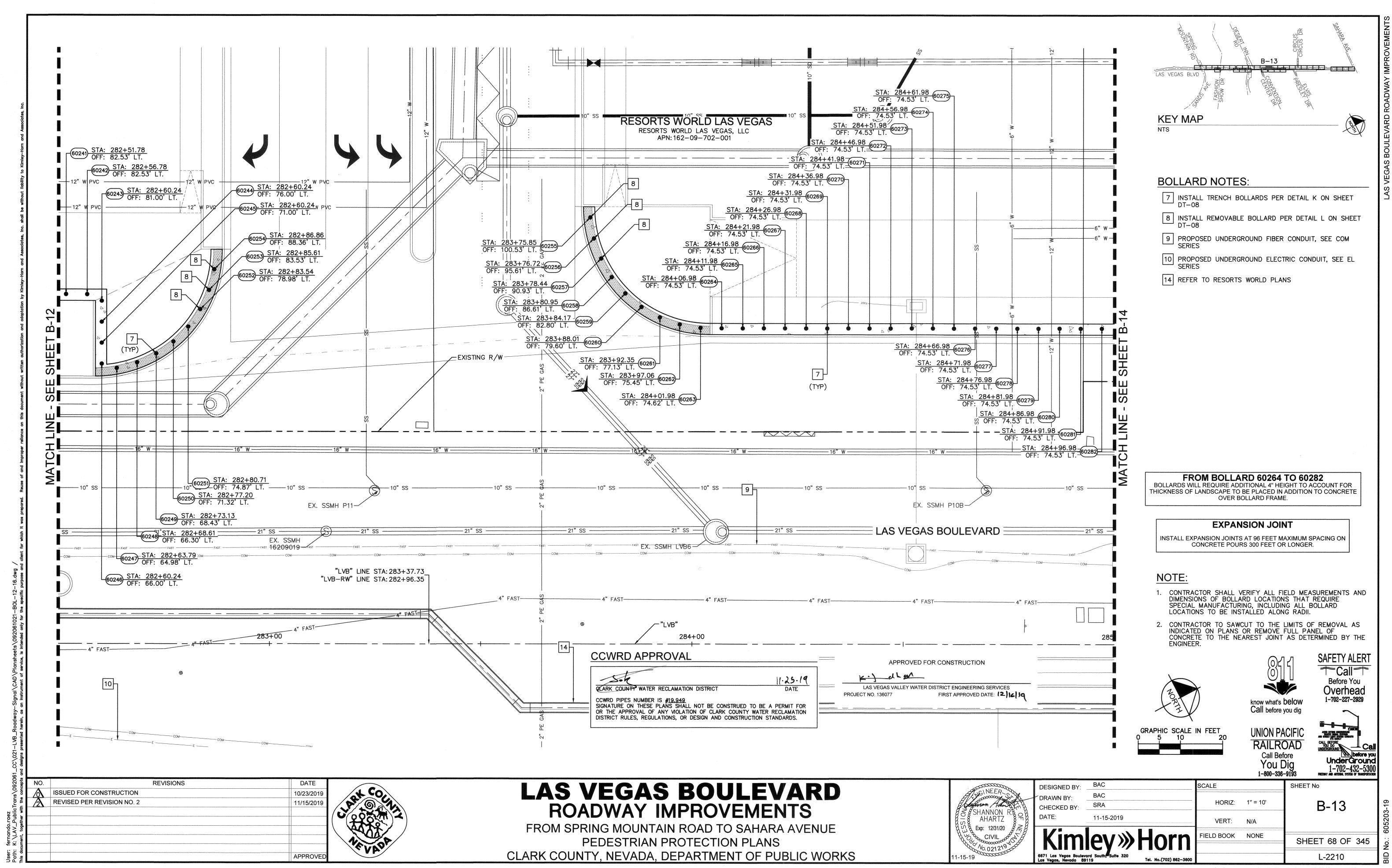
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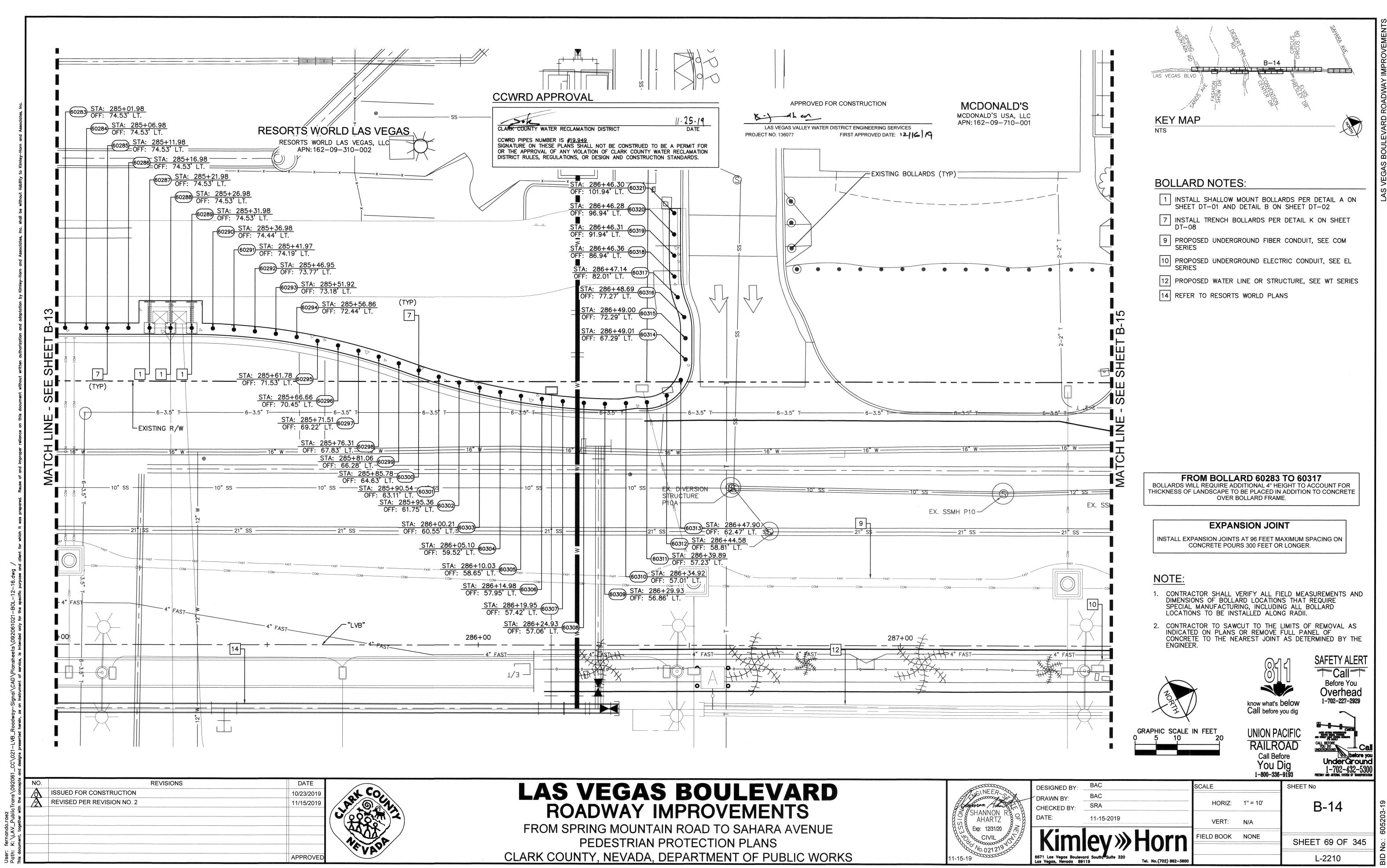


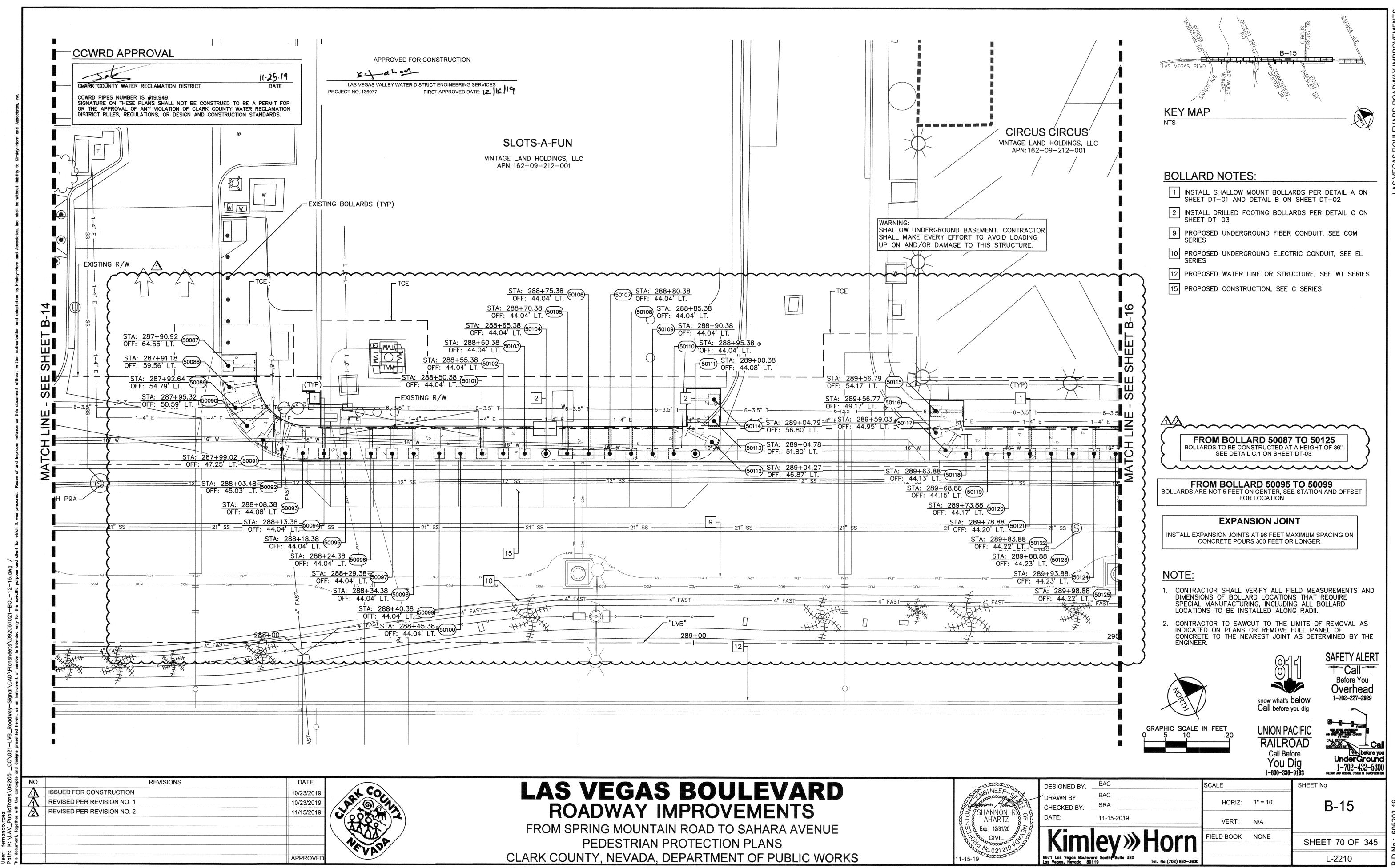


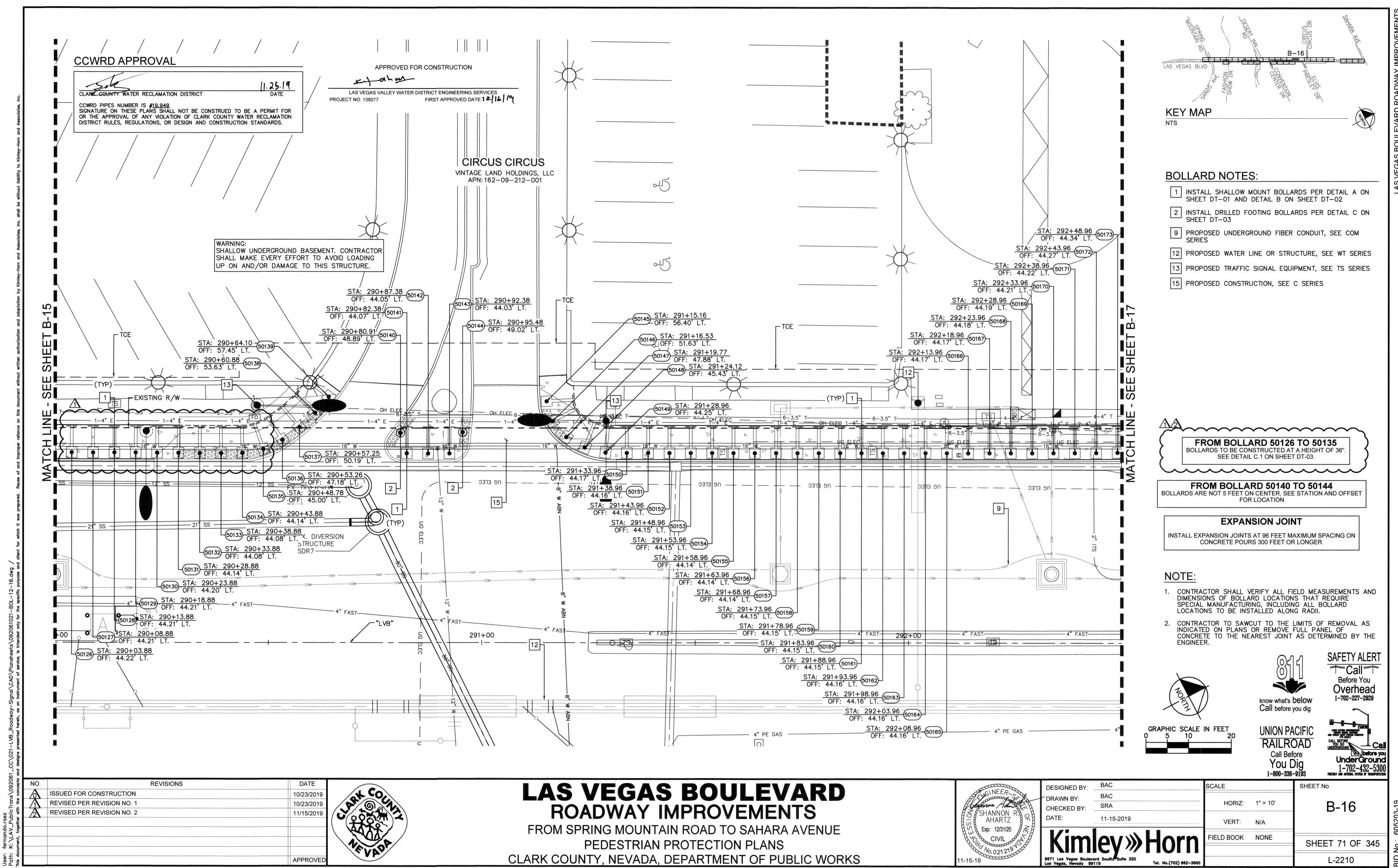


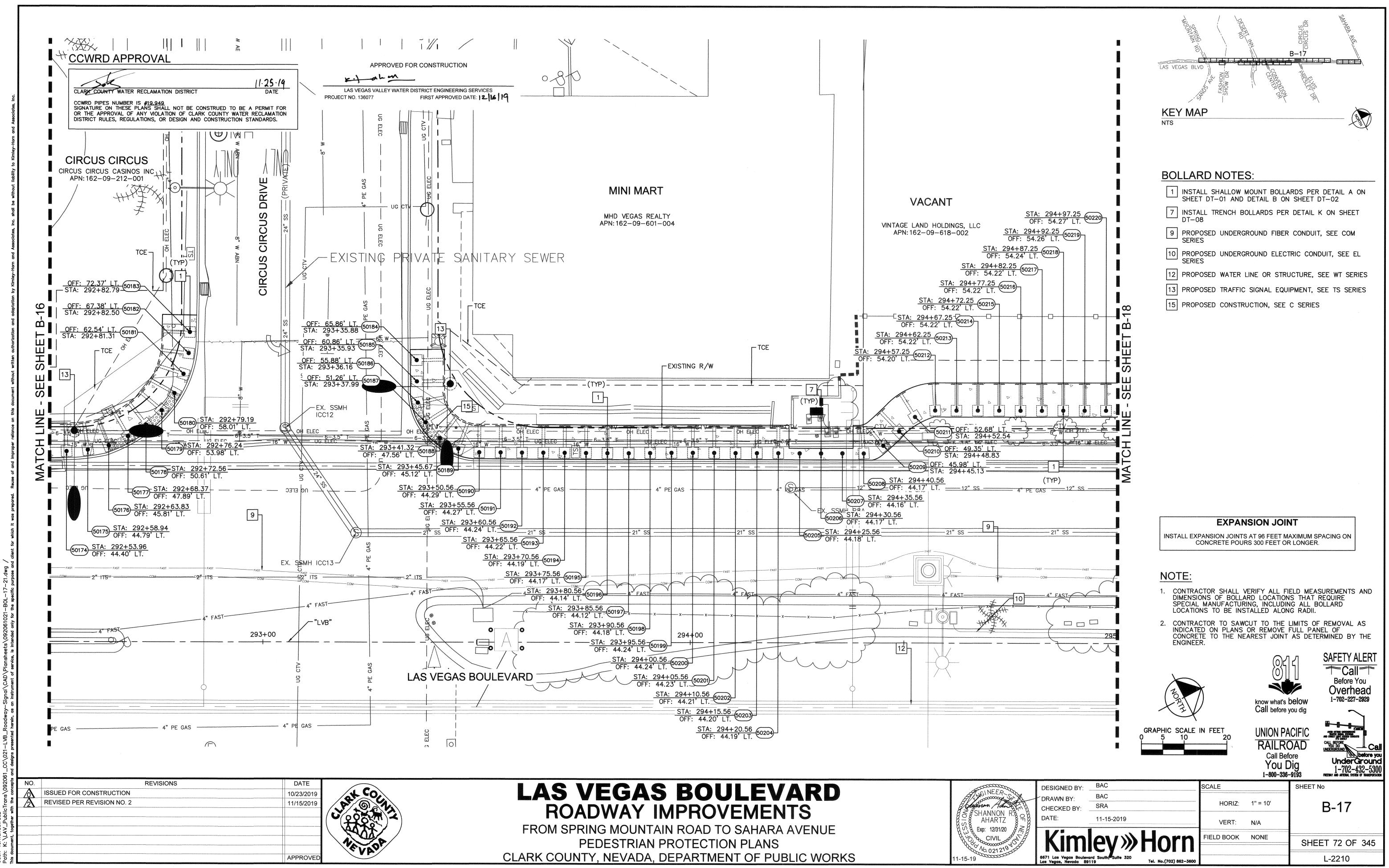


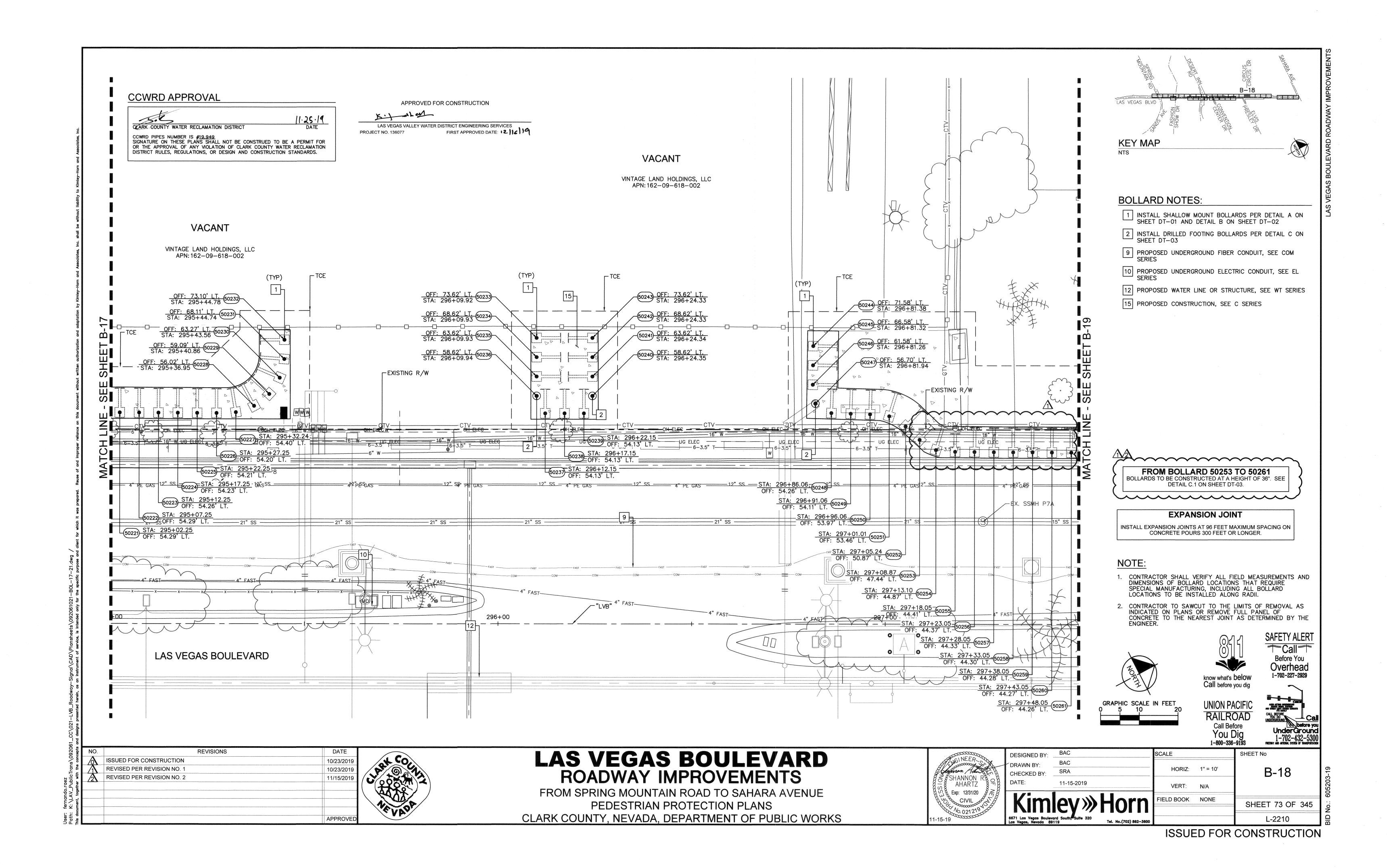


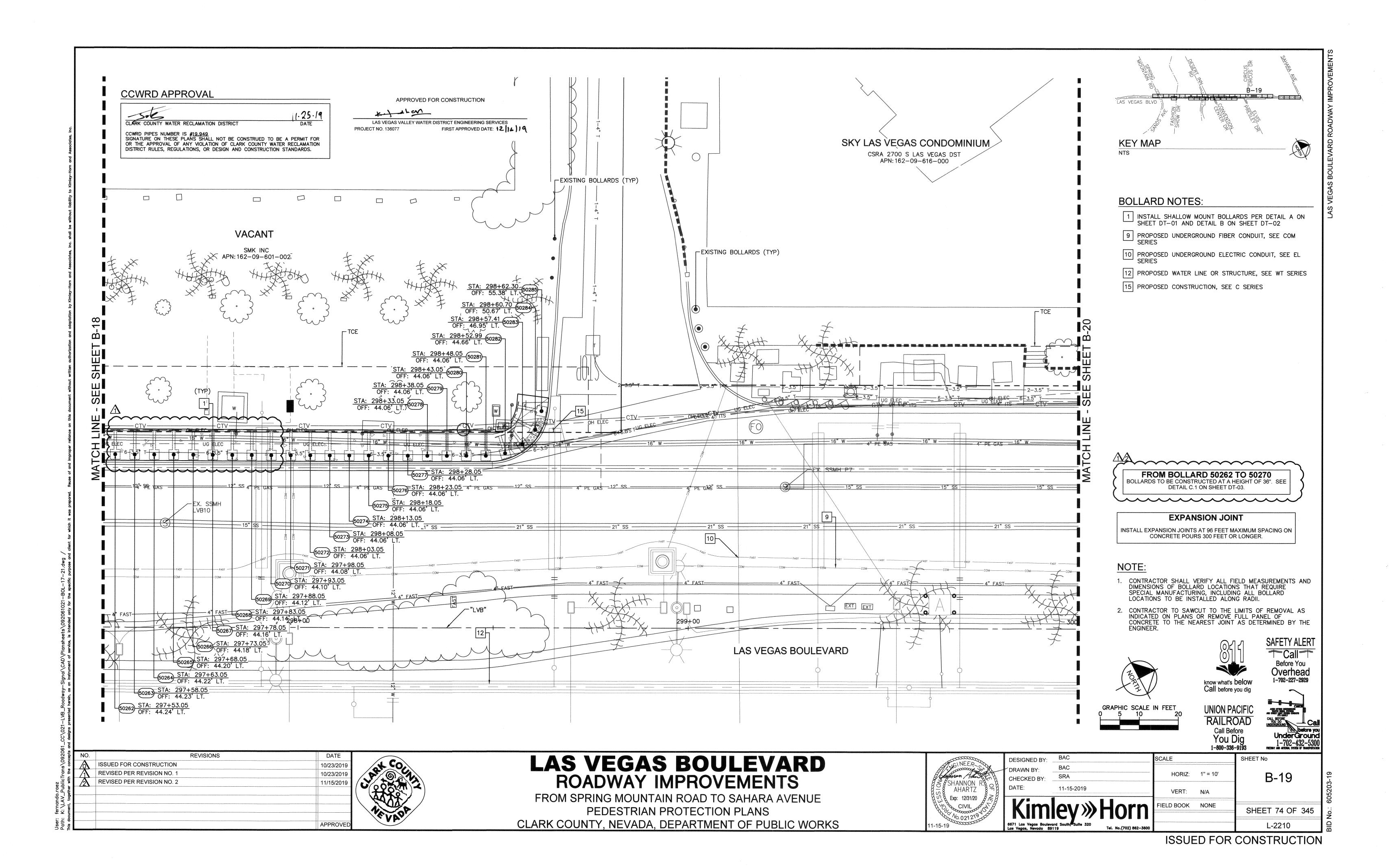


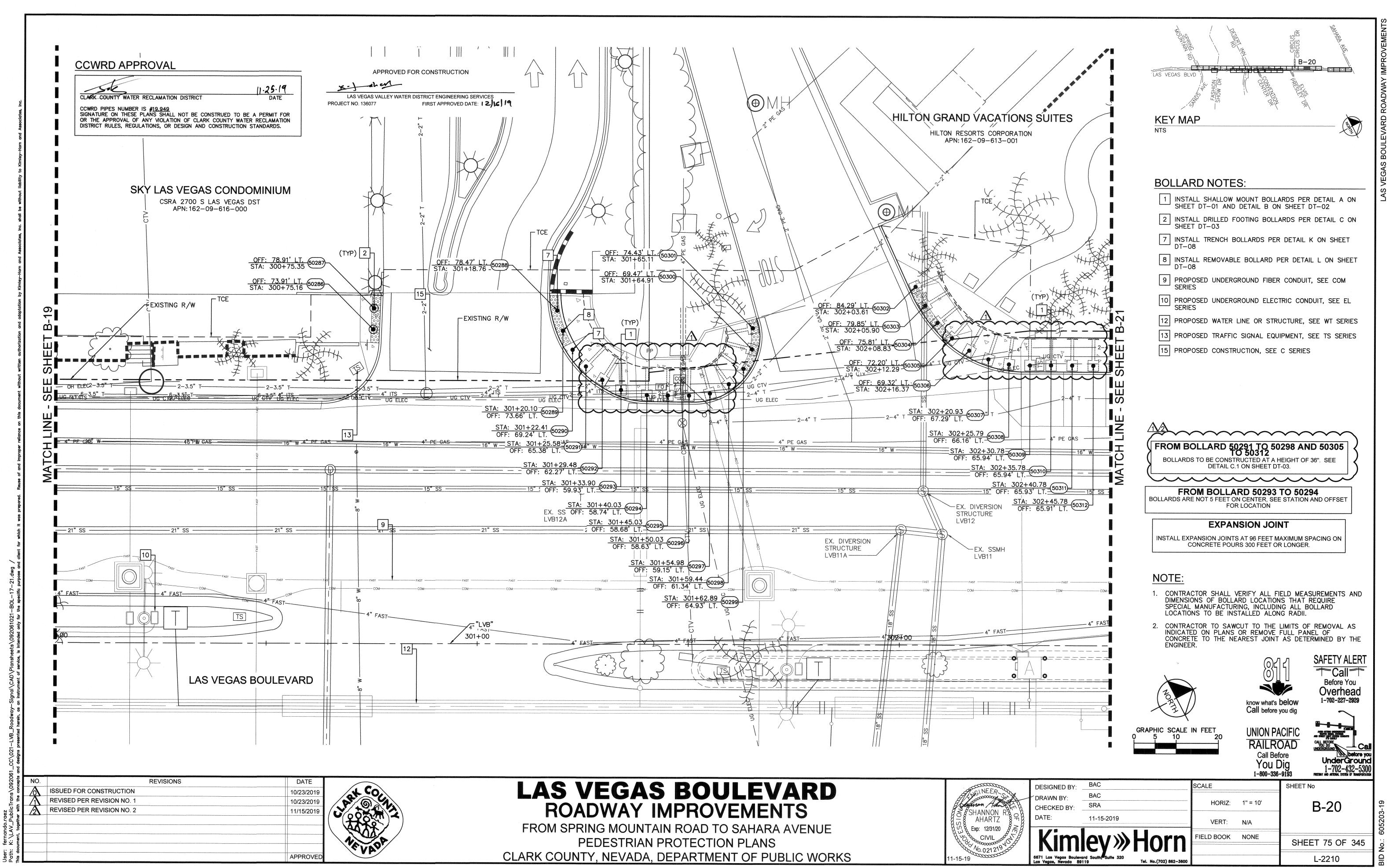


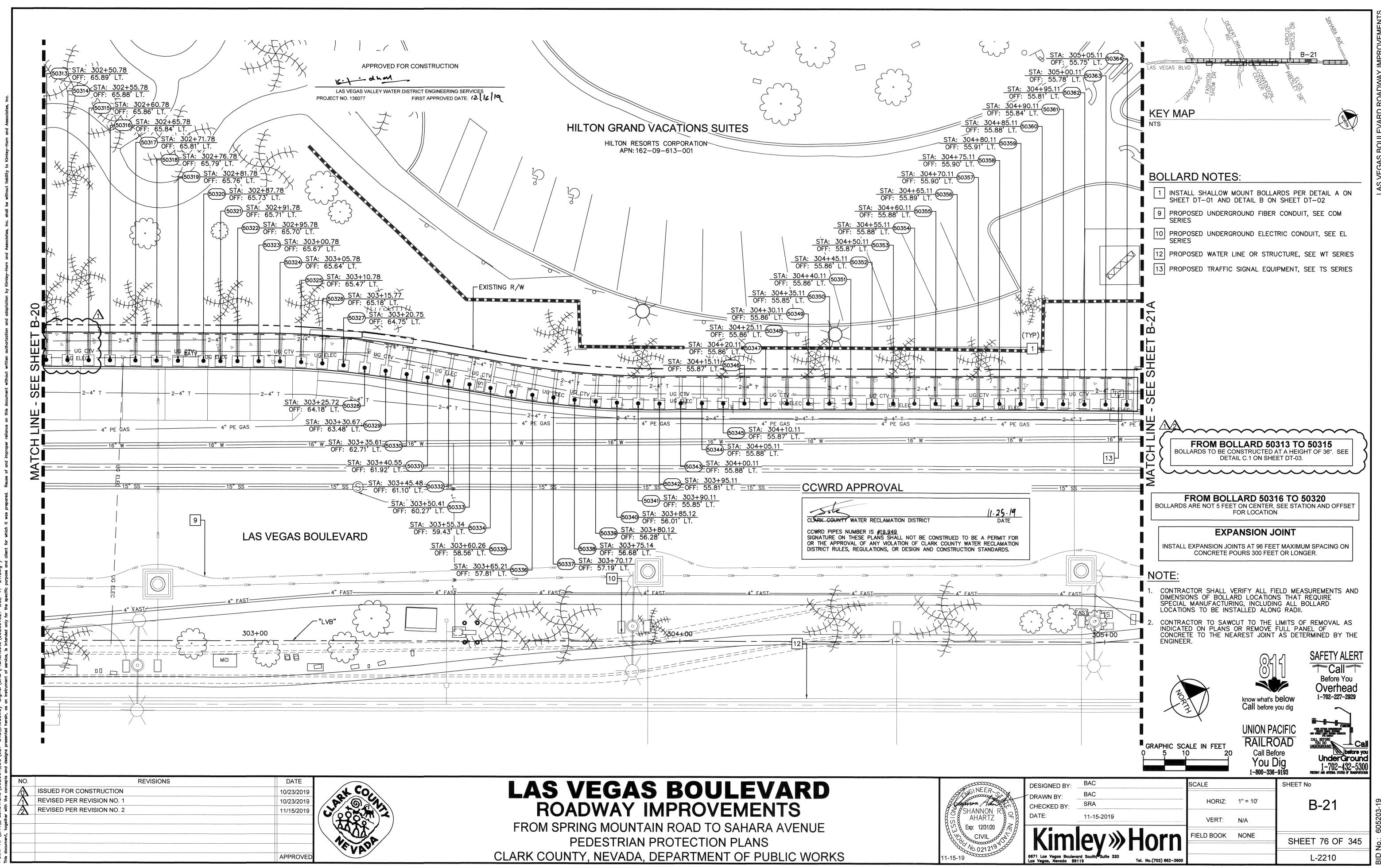


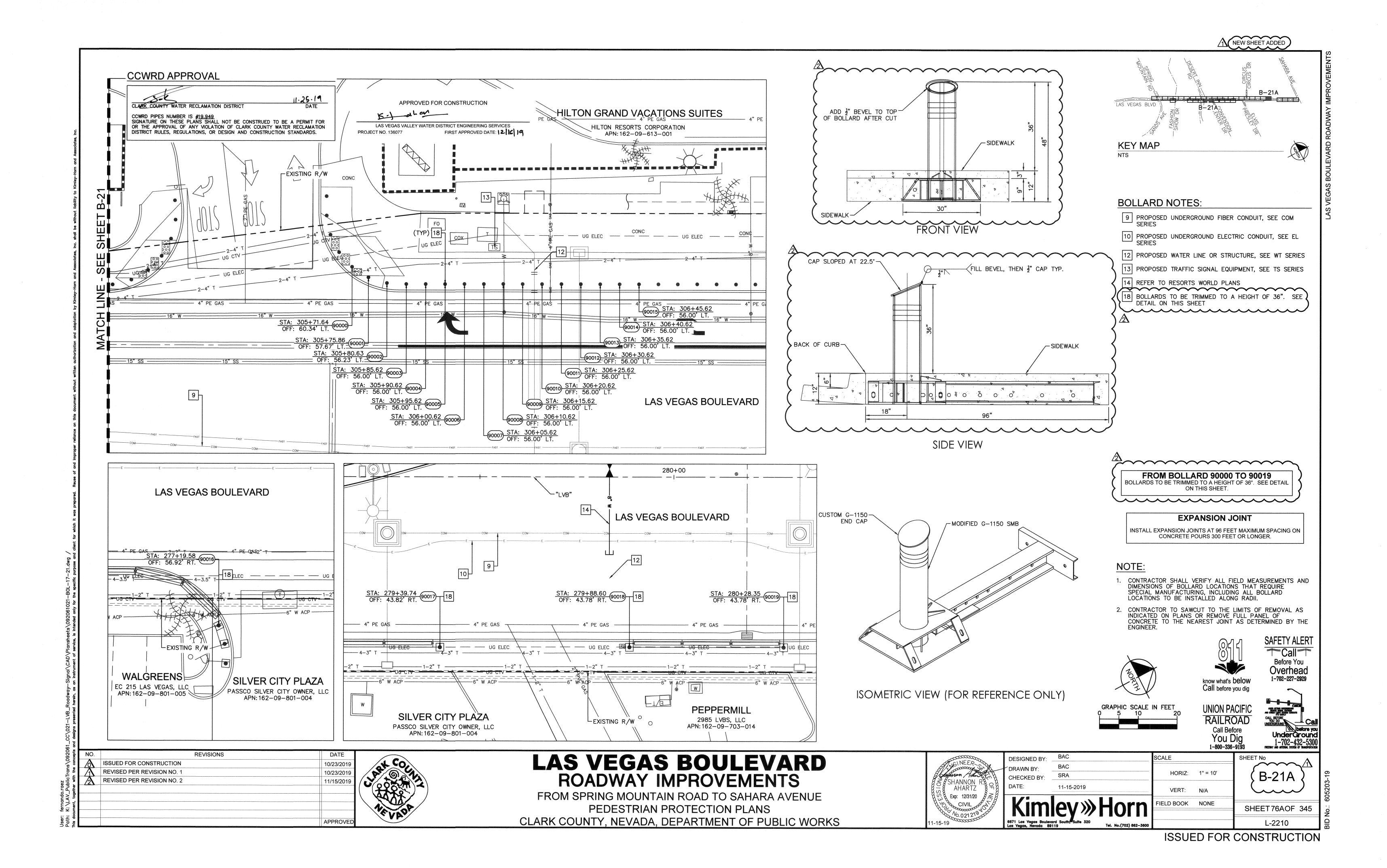


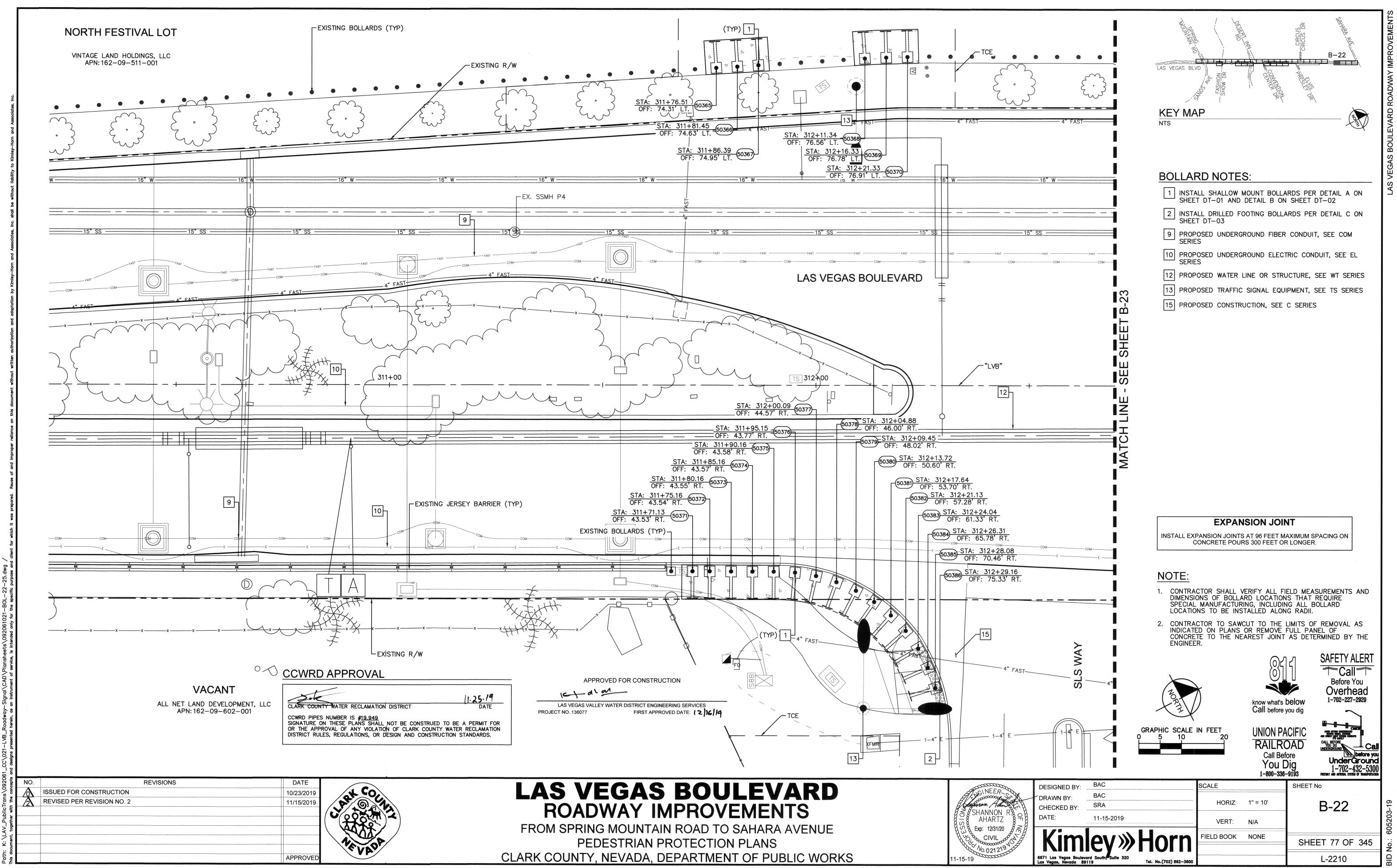


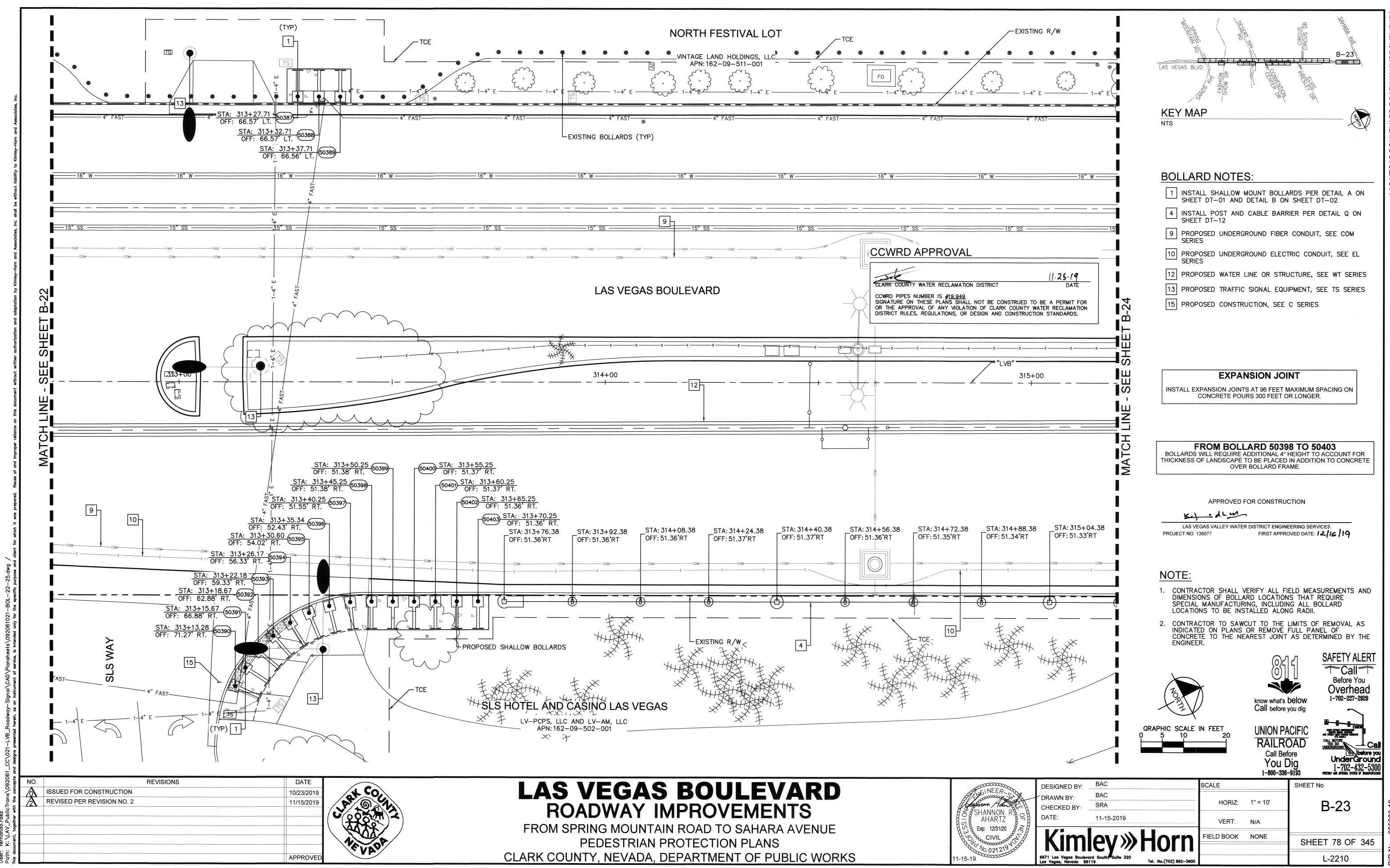


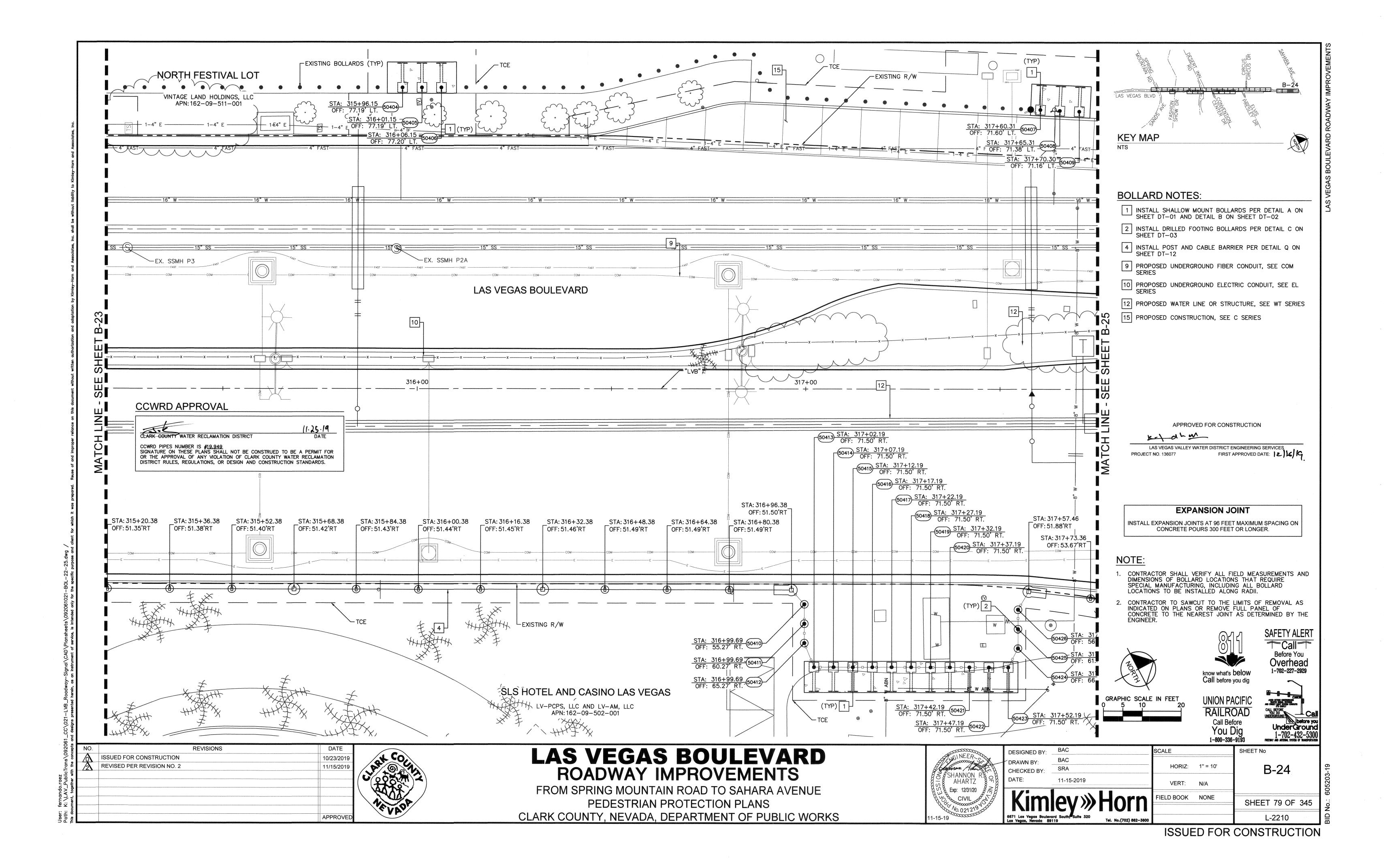


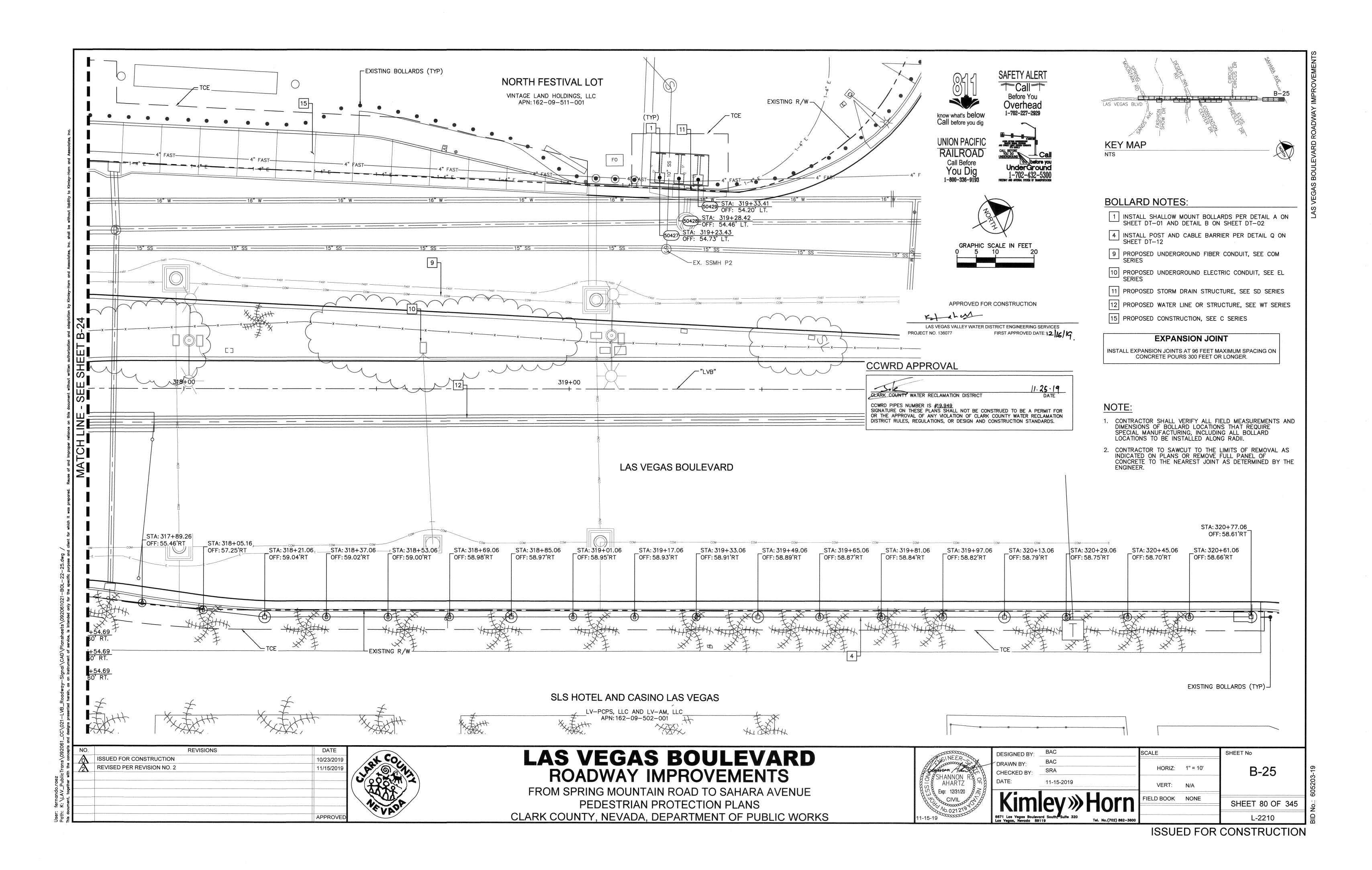


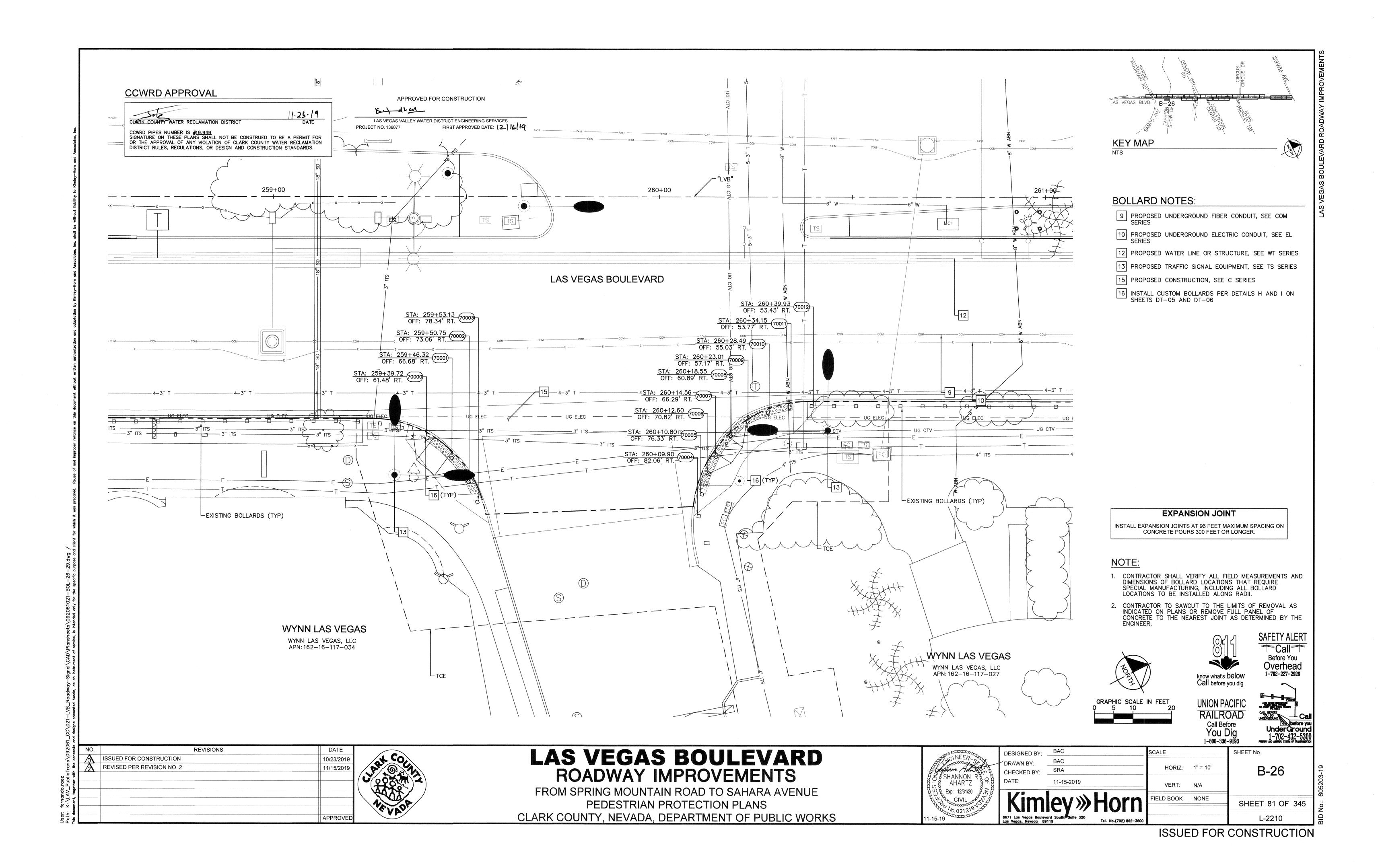


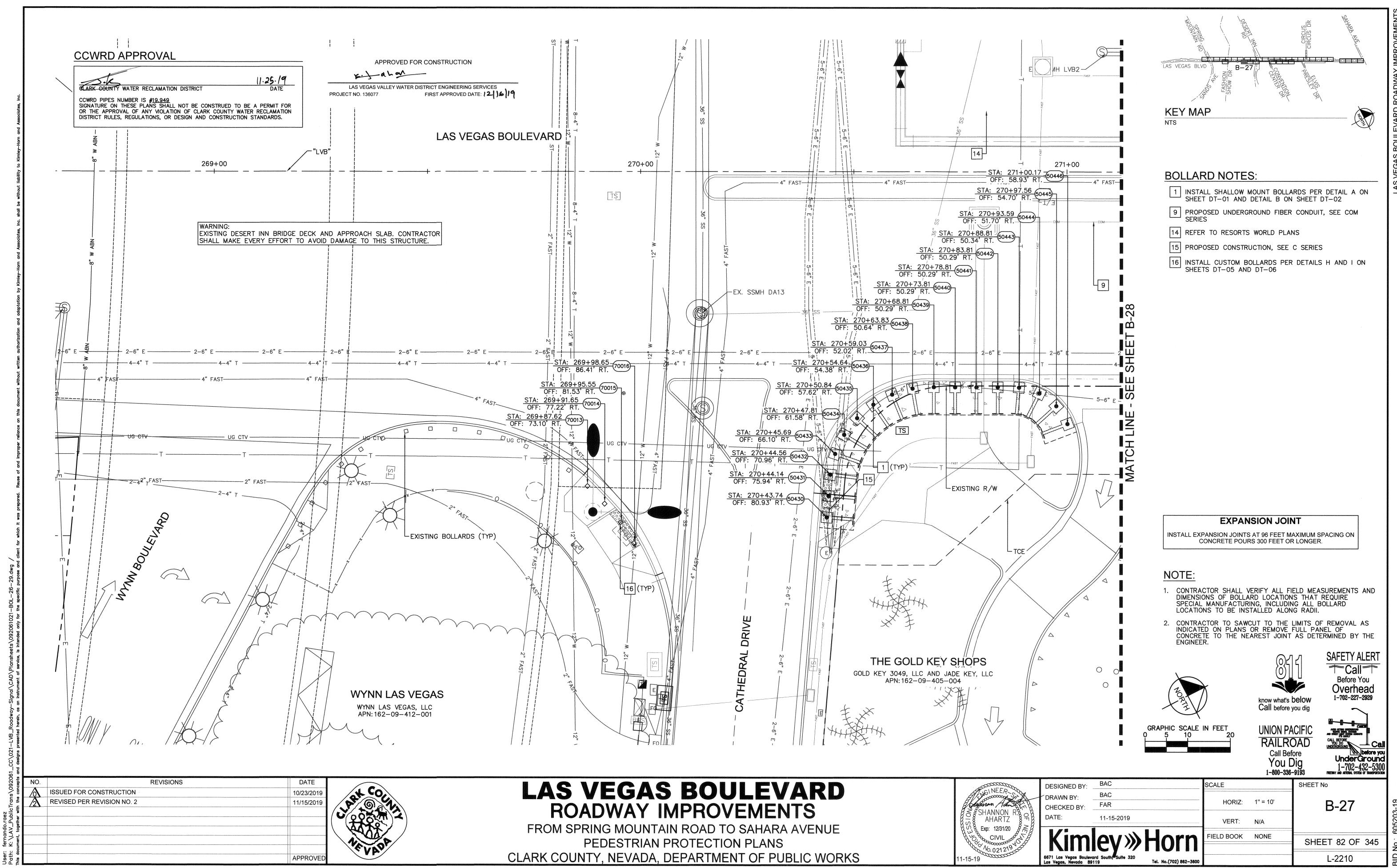


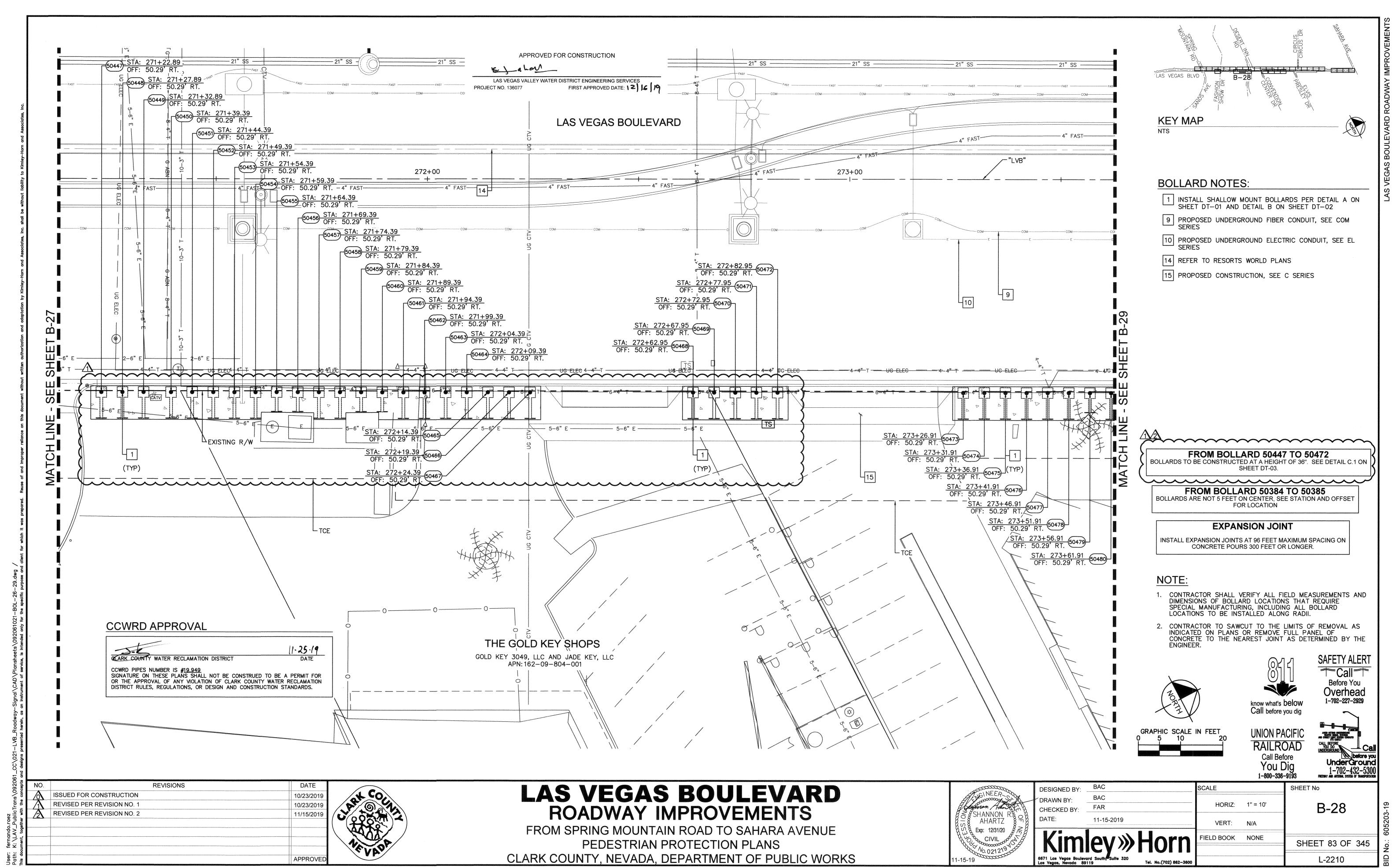


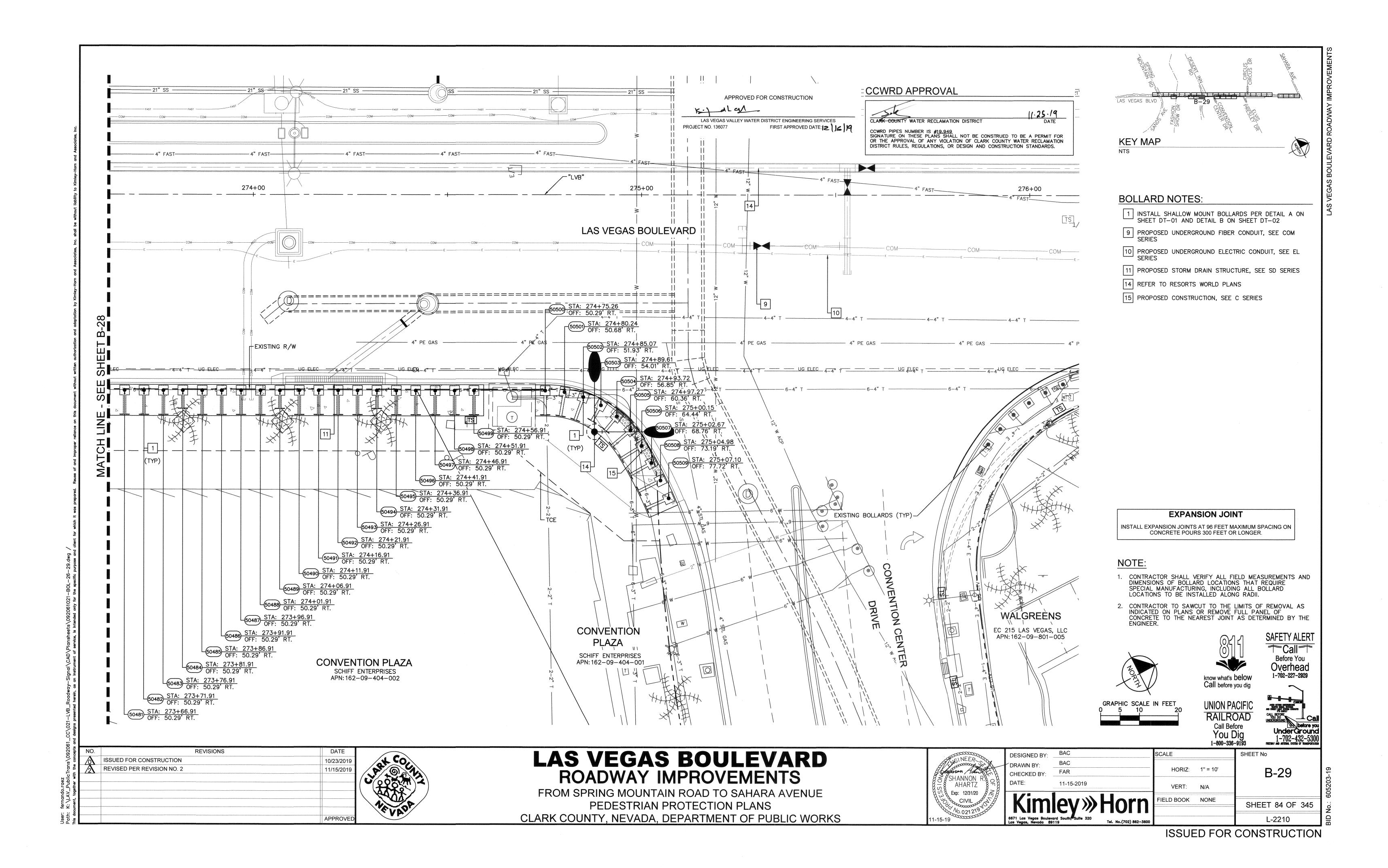


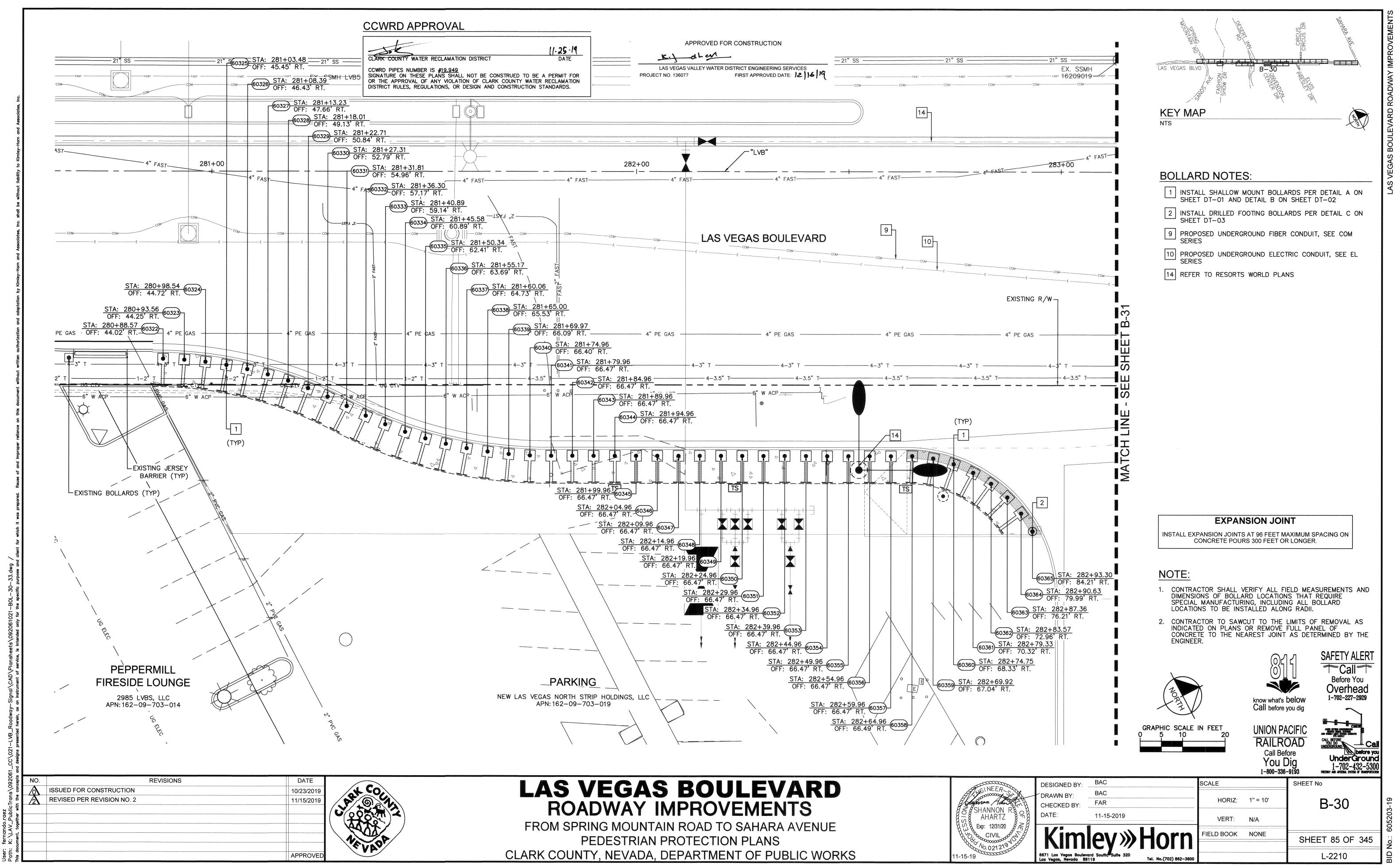


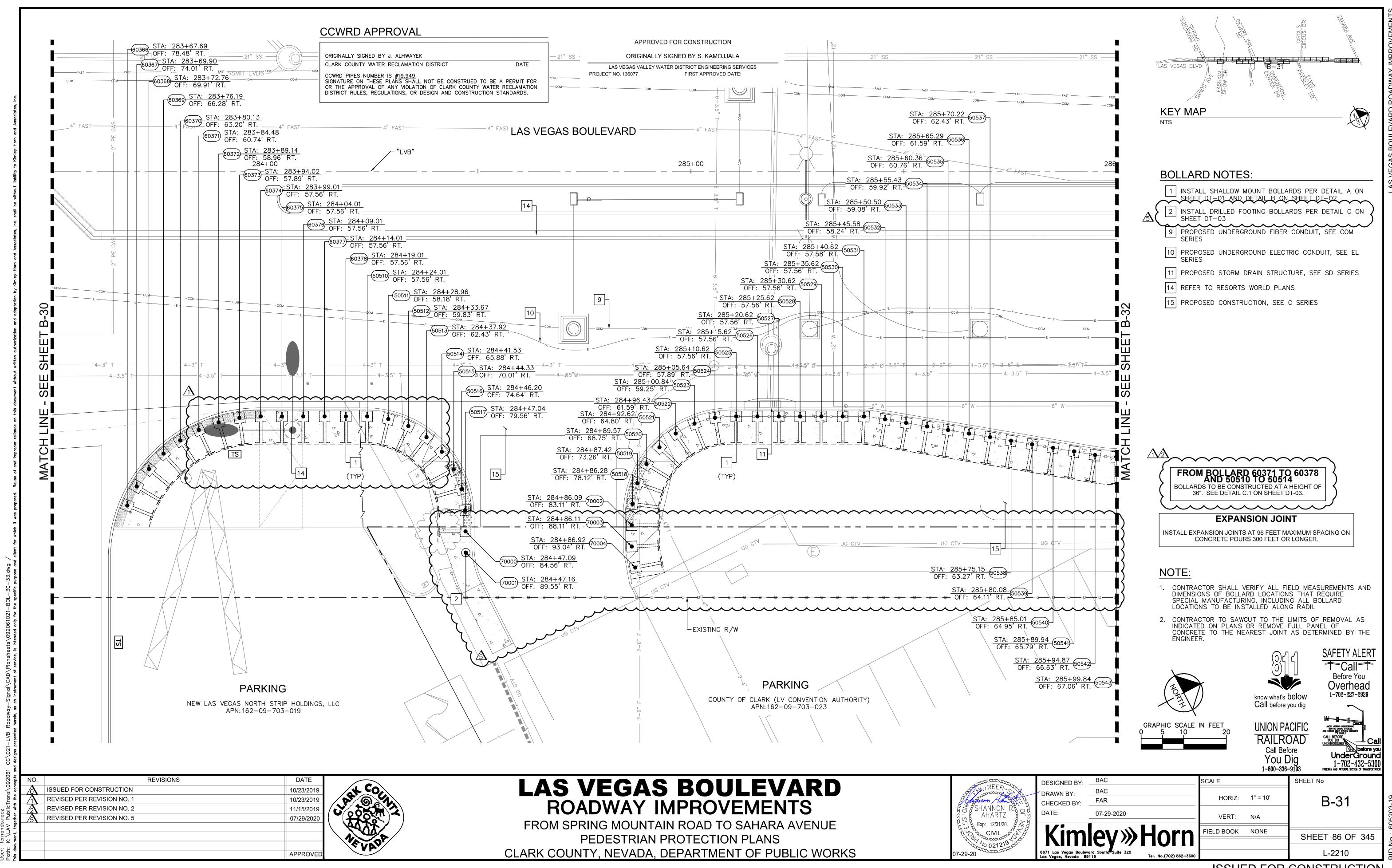


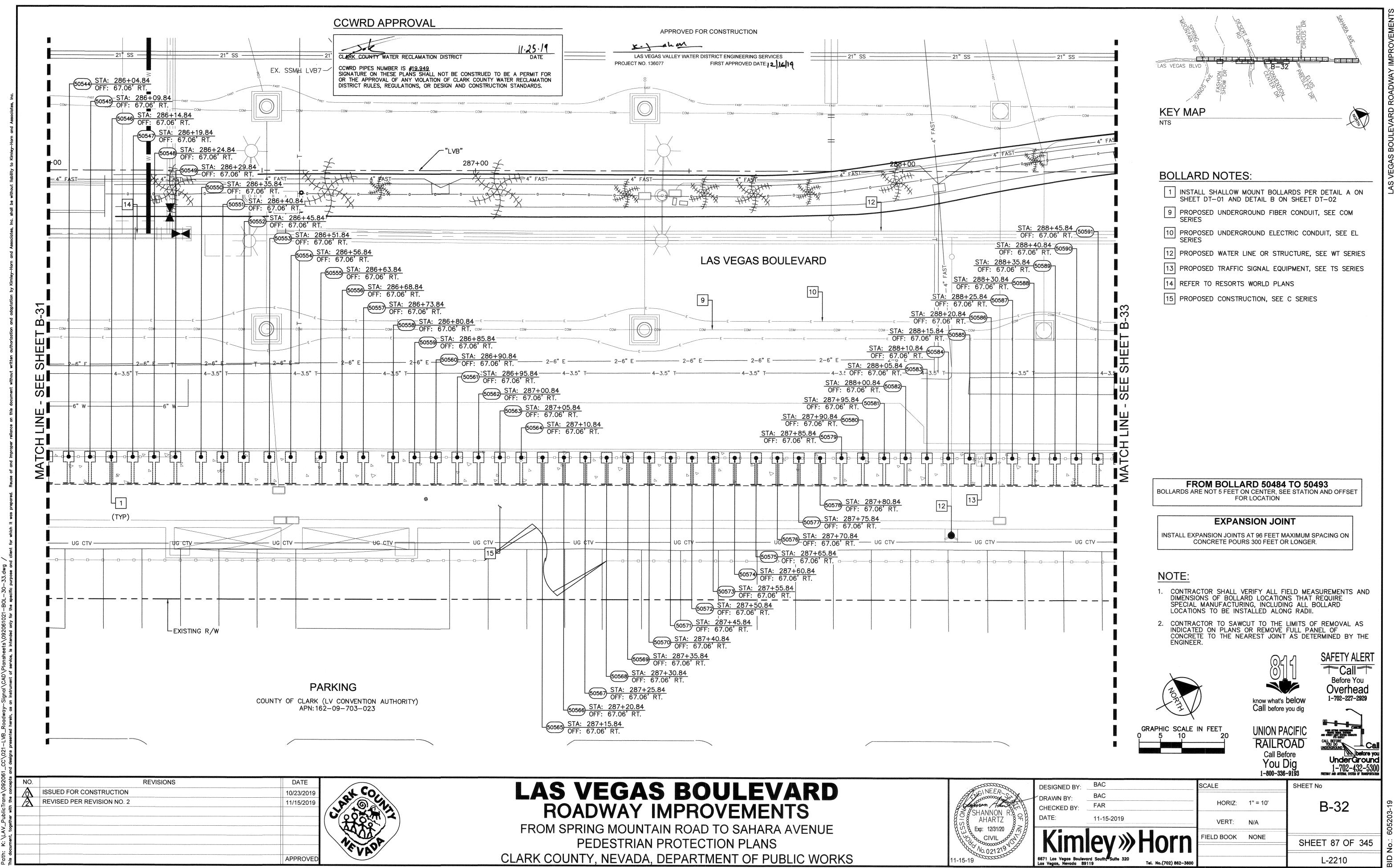


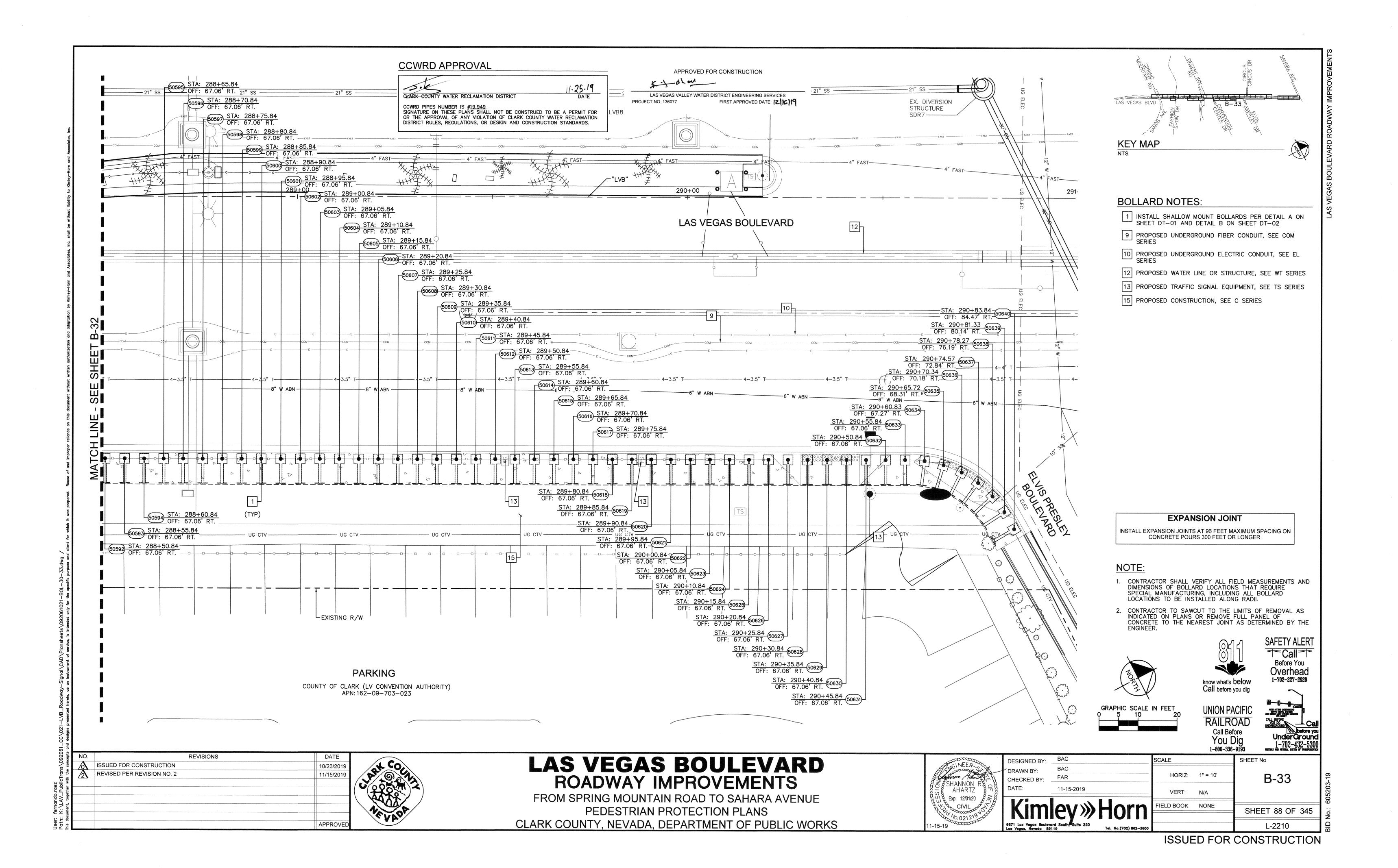


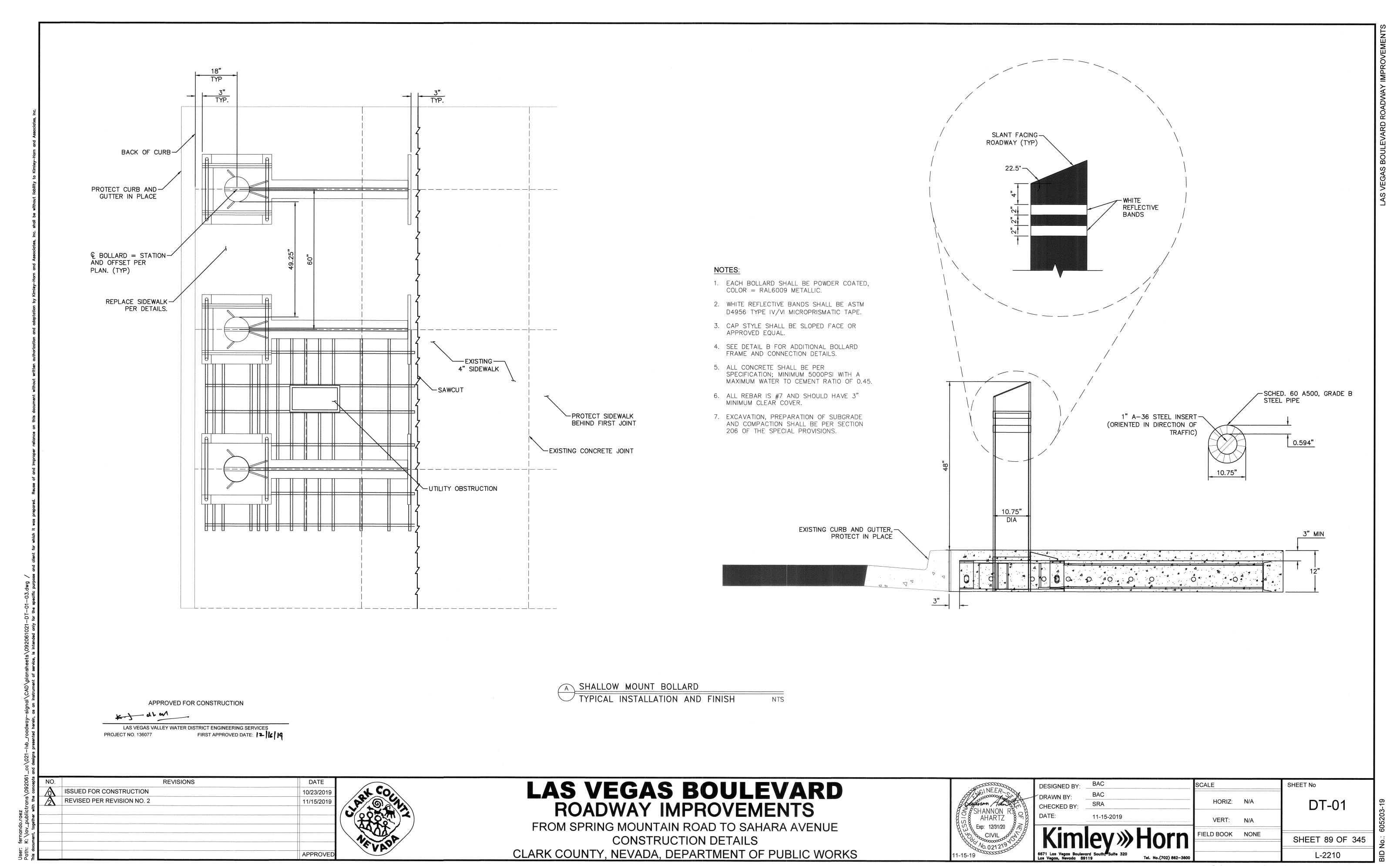




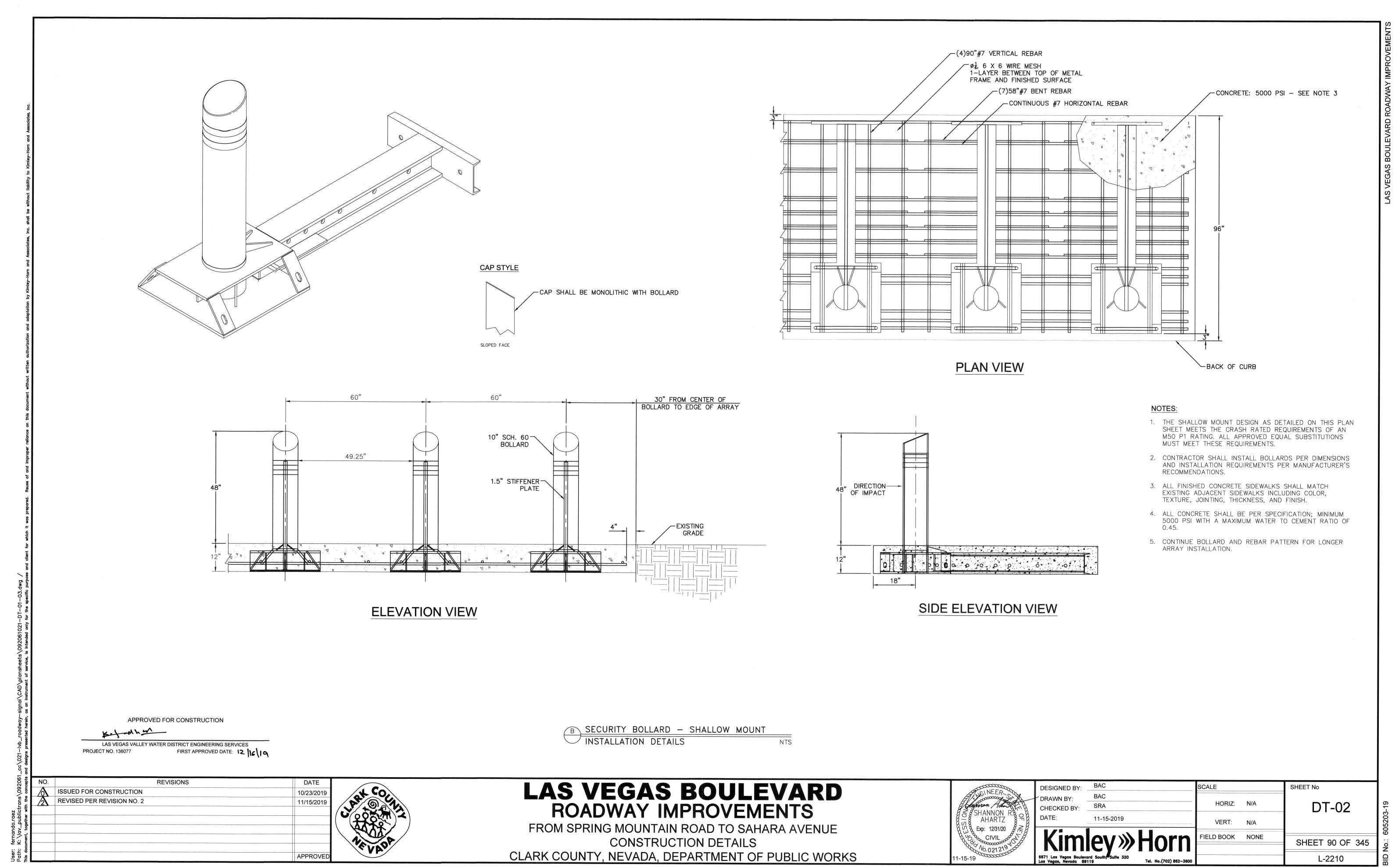


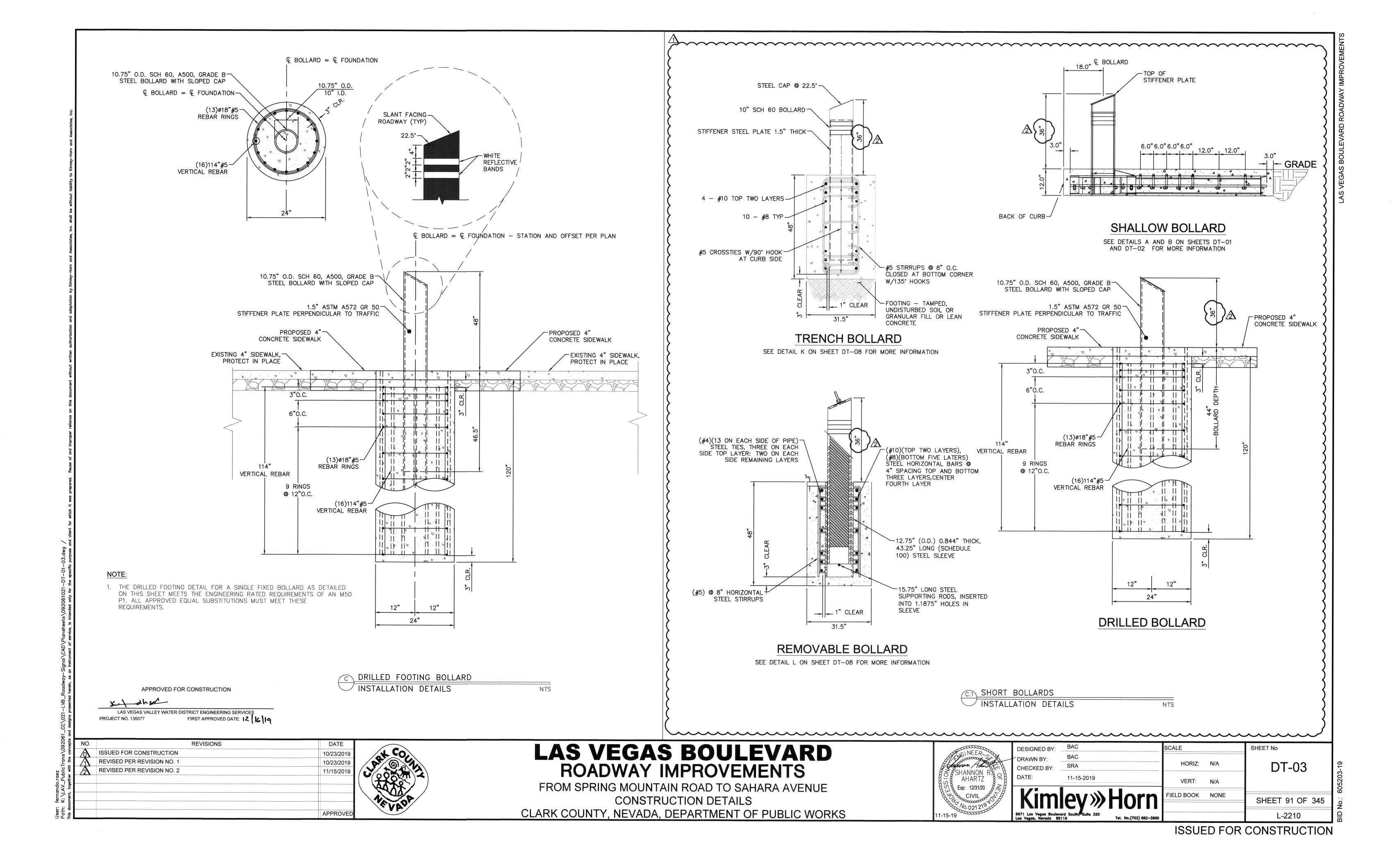


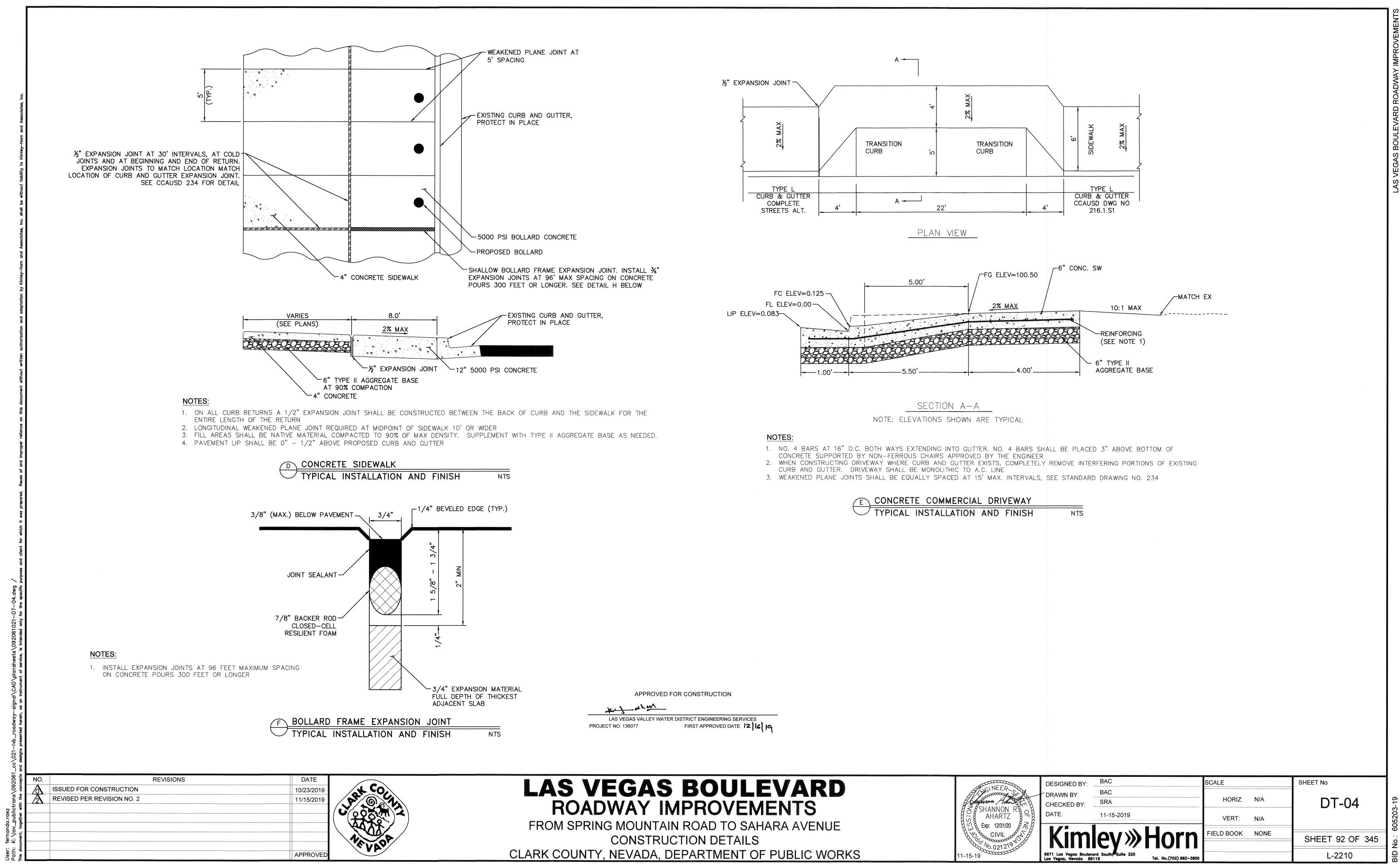


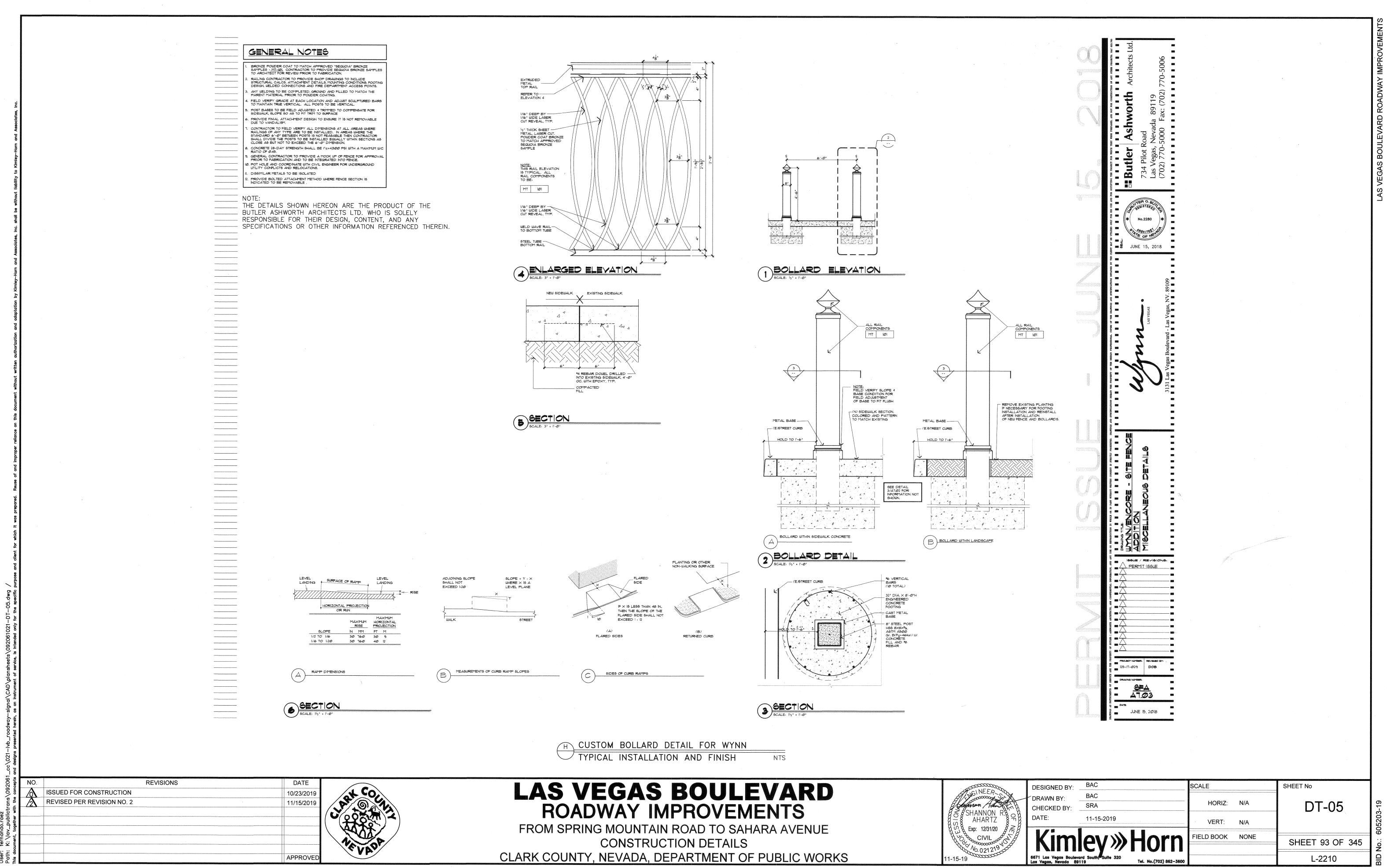


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## STRUCTURAL GENERAL NOTES

- ALL WORK SHALL CONFORM TO THE 2012 INTERNATIONAL BUILDING CODE AS AMENDED BY THE COUNTY OF LAS VEGAS.
- STRUCTURAL DRAWINGS REPRESENT THE FINISHED STRUCTURE, AND DO NOT SPECIFY THE MEANS AND METHODS OF CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE ALL MEANS NECESSARY TO PROTECT THE STRUCTURE, AND ANY ADJACENT NEW OR EXISTING STRUCTURES DURING CONSTRUCTION. SUCH MEASURES SHALL INCLUDE, BUT NOT BE LIMITED TO BRACING AND SHORING FOR LOADS ACTING ON THE STRUCTURE DURING CONSTRUCTION. OBSERVATION BY THE STRUCTURAL ENGINEER DURING CONSTRUCTION WILL NOT INCLUDE INSPECTION OF AFOREMENTIONED BRACING AND SHORING.
- EXISTING CONDITIONS ARE SHOWN TO THE BEST OF OUR KNOWLEDGE, DISCREPANCIES SHALL PROMPTLY BE REPORTED TO THE ENGINEER AND BE RESOLVED BEFORE PROCEEDING WITH THE WORK.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UTILITIES, WHICH MAY BE AFFECTED BY ITS WORK. INTERFERENCES WITH THE STRUCTURE SHALL PROMPTLY BE REPORTED TO THE ARCHITECT/ENGINEER AND BE RESOLVED BEFORE PROCEEDING WITH THE WORK.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COORDINATING THE WORK OF ALL TRADES AND VERIFYING ALL DIMENSIONS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ALL STRUCTURAL DISCREPANCIES, AND THESE DISCREPANCIES SHALL BE RESOLVED PRIOR TO PROCEEDING WITH THE WORK.
- SHOULD A DISCREPANCY OCCUR ON THE DRAWINGS BETWEEN ANY PROJECT SPECIAL NOTES/SPECIAL DETAILS, AND THE TYPICAL SPECS/TYPICAL DETAILS, SAID SPECIAL NOTES/SPECIAL DETAILS SHALL TAKE PRECEDENCE. DETAILS NOTED AS TYPICAL SHALL APPLY IN ALL CONDITIONS UNLESS SPECIFICALLY SHOWN OR NOTED.
- PENETRATIONS AND OPENINGS WITH ANY DIMENSION GREATER THAN 2" THAT ARE NOT SHOWN ON THE STRUCTURAL DRAWINGS ARE PROHIBITED UNLESS APPROVED IN WRITING BY THE STRUCTURAL ENGINEER. NO PENETRATION SHALL BE ALLOWED THROUGH ANY STRUCTURAL MEMBER WITHOUT THE APPROVAL OF THE ENGINEER.
- ANY CONSTRUCTION MATERIAL THAT IS TEMPORARILY PLACED ON FLOOR AND/OR ROOF FRAMING SHALL BE DISTRIBUTED OVER THE FRAMING SYSTEM SUCH THAT THE CONSTRUCTION LOAD DOES NOT EXCEED THE LOAD THAT THE FRAMING SYSTEM WAS DESIGNED FOR.
- DESIGN CRITERIA -
- A. DESIGN LIVE LOADS VEHICLE IMPACT = 1 KIP IN ANY DIRECTION BASIC WIND SPEED EXPOSURE CATEGORY PRIMARY FRAME DESIGN METHOD METHOD 2 (ANALYTICAL) BUILDING CLASSIFICATION ENCLOSED OCCUPANCY CATEGORY SITE CLASS
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF THE ADJACENT PROPERTIES, STRUCTURES, STREETS AND UTILITIES DURING THE CONSTRUCTION PERIOD. THE GENERAL CONTRACTOR AND ITS SUBCONTRACTORS MUST SUBMIT IN WRITING ANY

#### CONCRETE NOTES

- ALL CONCRETE UNLESS OTHERWISE NOTED SHALL BE REGULAR WEIGHT HARD ROCK TYPE (150#/CU.FT.).
- ALL PHASES OF WORK PERTAINING TO THE CONCRETE CONSTRUCTION SHALL CONFORM TO THE "BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE" (ACI 318-05) WITH MODIFICATIONS AS NOTED IN THE DRAWINGS OR SPECIFICATIONS.
- SCHEDULE OF STRUCTURAL CONCRETE 28-DAY STRENGTH AND TYPES: LOCATION OF STRUCTURE
  - 2500 PSI
- PORTLAND CEMENT SHALL CONFORM TO ASTM C-150 TYPE II.
- AGGREGATE FOR HARDROCK CONCRETE SHALL CONFORM TO ALL REQUIREMENTS AND TESTS OF ASTM C-33 AND PROJECT SPECIFICATIONS.
- CONCRETE MIXES SHALL BE DESIGNED BY A QUALIFIED TESTING LABORATORY AND SHALL BE SUBMITTED TO THE STRUCTURAL ENGINEER FOR HIS REVIEW 2 WEEKS
- CONCRETE MIXING OPERATION, ETC. SHALL CONFORM TO ASTM C-94.
- PLACEMENT OF CONCRETE SHALL CONFORM TO ACI STANDARD 301 AND PROJECT
- UNLESS OTHERWISE NOTED ON THE PLANS, MINIMUM CLEAR COVERAGE OF NEW CONCRETE OVER OUTER REINFORCING BARS SHALL BE AS FOLLOWS: A. CONCRETE POURED DIRECTLY
- AGAINST EARTH..... ......3" CLEAR TO REINFORCING B. WALL FACES: EXPOSED TO EARTH WITH

STRUCTURAL SLABS....

- FORMED SURFACES OR ..1-1/2" CLEAR FOR #5 BAR & SMALLER EXPOSED TO WEATHER ... 2" CLEAR FOR #6 BARS & LARGER INTERIOR FACES.... ...3/4 CLEAR
- C. BEAMS AND COLUMNS: NOT EXPOSED TO EARTH OR WEATHER..... .....1-1/2" CLEAR TO STIRRUPS & TIES FORMED AND EXPOSED TO EARTH OR WEATHER..... ....1 1/2" CLEAR TO STIRRUPS & TIES
- . ALL REINFORCING BARS, ANCHOR BOLTS AND OTHER CONCRETE INSERTS SHALL BE WELL SECURED IN POSITION PRIOR TO PLACING CONCRETE.
- PROJECTING CORNERS OF BEAMS, WALLS, COLUMNS, EQUIPMENT PADS, ETC., SHALL BE FORMED WITH 3/4" CHAMFER, UNLESS OTHERWISE NOTED ON ARCHITECTURAL

....1" CLEAR AT TOP AND BOTTOM

- PROVIDE SLEEVES FOR PLUMBING AND ELECTRICAL OPENINGS IN CONCRETE BEFORE PLACING. DO NOT CUT ANY REINFORCING WHICH MAY CONFLICT. CORING IN CONCRETE IS NOT PERMITTED EXCEPT AS SHOWN. NOTIFY THE STRUCTURAL ENGINEER IN ADVANCE OF CONDITIONS NOT SHOWN ON THE DRAWINGS.
- 13. CONDUIT OR PIPE SIZE (O.D.) THAT IS BURIED IN ANY CONCRETE SLABS SHALL NOT EXCEED 25 PERCENT OF SLAB THICKNESS AND SHALL BE PLACED BETWEEN THE TOP CONCENTRATIONS OF CONDUITS OR PIPES SHALL BE AVOIDED EXCEPT WHERE DETAILED
- 14. DO NOT USE CONCRETE ADMIXTURES CONTAINING CHLORIDE OR CHLORIDE SALTS. 15. ALL ROUGHENED SURFACES IN CONCRETE SHALL BE MADE WITH A MINIMUM AMPLITUDE OF 1/4"
- 16. NO ADDITIONAL WATER IS ALLOWED IN THE CONCRETE MIX AT THE SITE.

### REINFORCING STEEL NOTES

- ALL REINFORCING STEEL SHALL BE DETAILED AND PLACED IN CONFORMANCE WITH THE "BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE" (ACI 318-05), THE CRSI "MANUAL OF STANDARD PRACTICE," AND THE "ACI DETAILING MANUAL (SP-66) AS MODIFIED BY THE PROJECT DRAWINGS AND SPECIFICATIONS.
- REINFORCING BARS SHALL CONFORM TO ASTM A-615 GRADE 60 REQUIREMENTS. #4 AND SMALLER BARS MAY BE GRADE 40.
- ANCHOR BOLTS, DOWELS AND OTHER EMBEDDED ITEMS ARE TO BE SECURELY TIED IN PLACE BEFORE CONCRETE IS POURED.
- 4. ALL REINFORCING BAR BENDS SHALL BE MADE COLD.
- REINFORCING SPLICES SHALL BE MADE ONLY WHERE INDICATED ON THE DRAWINGS.
- DOWELS BETWEEN FOOTING AND WALL OR COLUMNS SHALL BE THE SAME GRADE, SIZE, SPACING, AND NUMBER AS THE VERTICAL REINFORCING RESPECTIVELY, U.O.N.
- WELDING OF REINFORCING STEEL IS NOT PERMITTED UNLESS OTHERWISE SHOWN ON CONTRACTOR SHALL SUBMIT REINFORCING BAR LAYOUTS AND DETAILS FOR ARCHITECT'S
- REVIEW PRIOR TO FABRICATION. FABRICATE FROM REVIEWED DRAWINGS ONLY. REINFORCING BARS SHALL BE AS LONG AS PRACTICABLE AND AS DETAILED AND SHALL BE LAPPED AT SPLICES AND CORNERS NOT LESS THAN 32 BAR DIAMETER (24" MINIMUM), UNLESS OTHERWISE SHOWN. STAGGER HORIZONTAL WALL BAR SPLICES. IN GENERAL, BAR SPLICES SHALL BE MADE AT POINTS OF MINIMUM STRESS. IN BEAMS AND SLABS, SPLICE TOP BARS AT MID-SPAN, BOTTOM BARS OVER SUPPORTS, UNLESS
- EMBEDDED METAL COMPONENTS MADE UP OF ALLOYS THAT ARE DIS-SIMILAR TO THAT OF THE REINFORCING STEEL SHALL NOT BE ATTACHED DIRECTLY TO REINFORCING. MEASURES SHALL BE TAKEN TO ELECTRICALLY ISOLATE SAID COMPONENTS FROM ANY REINFORCING TO PREVENT CATHODIC EFFECTS.

### FOUNDATION NOTES

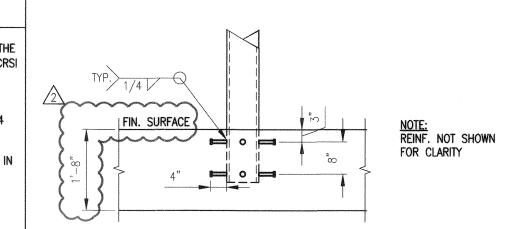
- THE FOUNDATION DESIGN WAS BASED ON THE ASSUMPTIONS PRESENTED BELOW IN THE ABSENCE OF A SOILS REPORT: ALLOWABLE SOIL BEARING PRESSURE ALLOWABLE PASSIVE EARTH RESISTANCE = 100 PCF FRICTIONAL RESISTANCE  $= 0.25 \times DEAD LOAD$
- . FOOTING BACKFILL AND UTILITY TRENCH BACKFILL WITHIN THE BUILDING AREA SHALL BE MECHANICALLY COMPACTED IN LAYERS. FLOODING IS PROHIBITED.
- CONTRACTOR SHALL PROVIDE FOR DE-WATERING OF EXCAVATIONS FROM EITHER SURFACE WATER, GROUND WATER, OR SEEPAGE.

4. SOIL DESIGN PARAMETERS CAN BE INCREASED BY 1/3 FOR IMPACT LOADS.

## SPECIAL INSPECTION NOTES

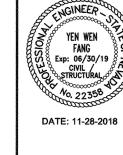
CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT SPECIAL INSPECTION OF PORTIONS OF THE WORK, AS REQUIRED BY THE BUILDING CODE OF THE COUNTY OF CLARK, BE MADE AT THE APPROPRIATE TIME. THE CONTRACTOR SHALL GIVE TIMELY NOTICE OF WHEN AND WHERE INSPECTIONS ARE TO BE MADE AND PROVIDE ACCESS FOR THE INSPECTOR. THE CONTRACTOR SHALL CORRECT DEFECTIVE WORK AT NO ADDITIONAL COST TO THE OWNER AND THE CONTRACTOR SHALL PAY FOR RE-INSPECTION.

THE FOLLOWING IS A SUMMARY OF THE SPECIAL INSPECTION REQUIREMENTS: NO PER IBC 1704.4.2.3, DESIGN CONCRETE BASED ON 2,500 PSI CONCRETE



NELSON STUD TO **HSS CONNECTION** 

FIN. SURFACE <



4775 W. Teco Ave. Ste.255

Las Vegas, Nevada 89118

Main (702) 931-3000

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Hawai`i | Las Vegas

\_\_\_\_3'-0"x5'-0" FTG. x 20" THK. MIN. ₩/ #5 🗖 @ 12" O.C. T&B E.W.

POST AND COLLAR PER OTHERS

HSS 8x8x5/8 POST EXTEND INTO CONC. FOOTING W/ (2)-3/4"ø x 4" LONG NELSON STUDS ON EA. SIDE OF HSS (TOTAL OF 8).

FOOTING CAN BE ROTATED 90° OR MADE CONTINUOUS IF REQUIRED BY FIELD CONDITIONS. 3'-0"

FOOTING DETAIL

THE DETAILS SHOWN HEREON ARE THE PRODUCT OF THE ENGINEERING PARTNERS WHO IS SOLELY RESPONSIBLE FOR THEIR DESIGN, CONTENT, AND ANY SPECIFICATIONS OR OTHER INFORMATION REFERENCED THEREIN.

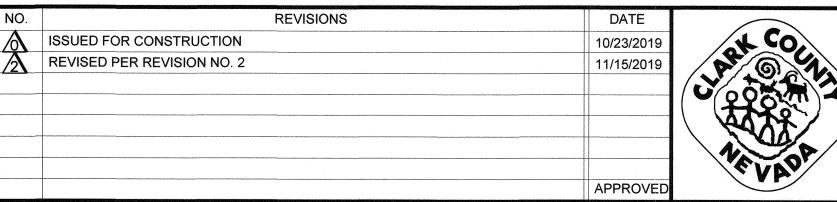
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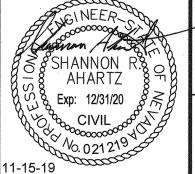
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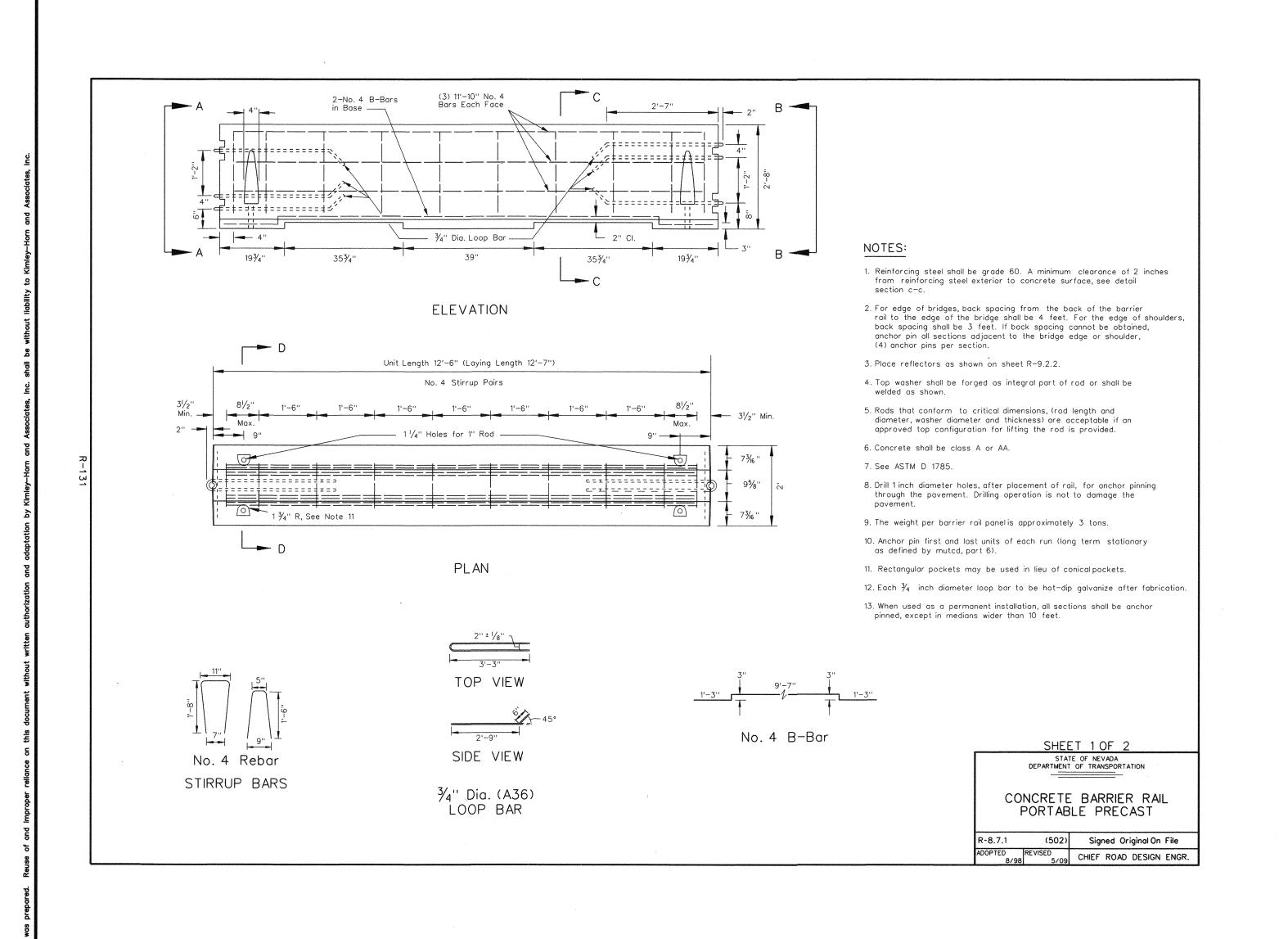
## LAS VEGAS BOULEVARD ROADWAY IMPROVEMENTS

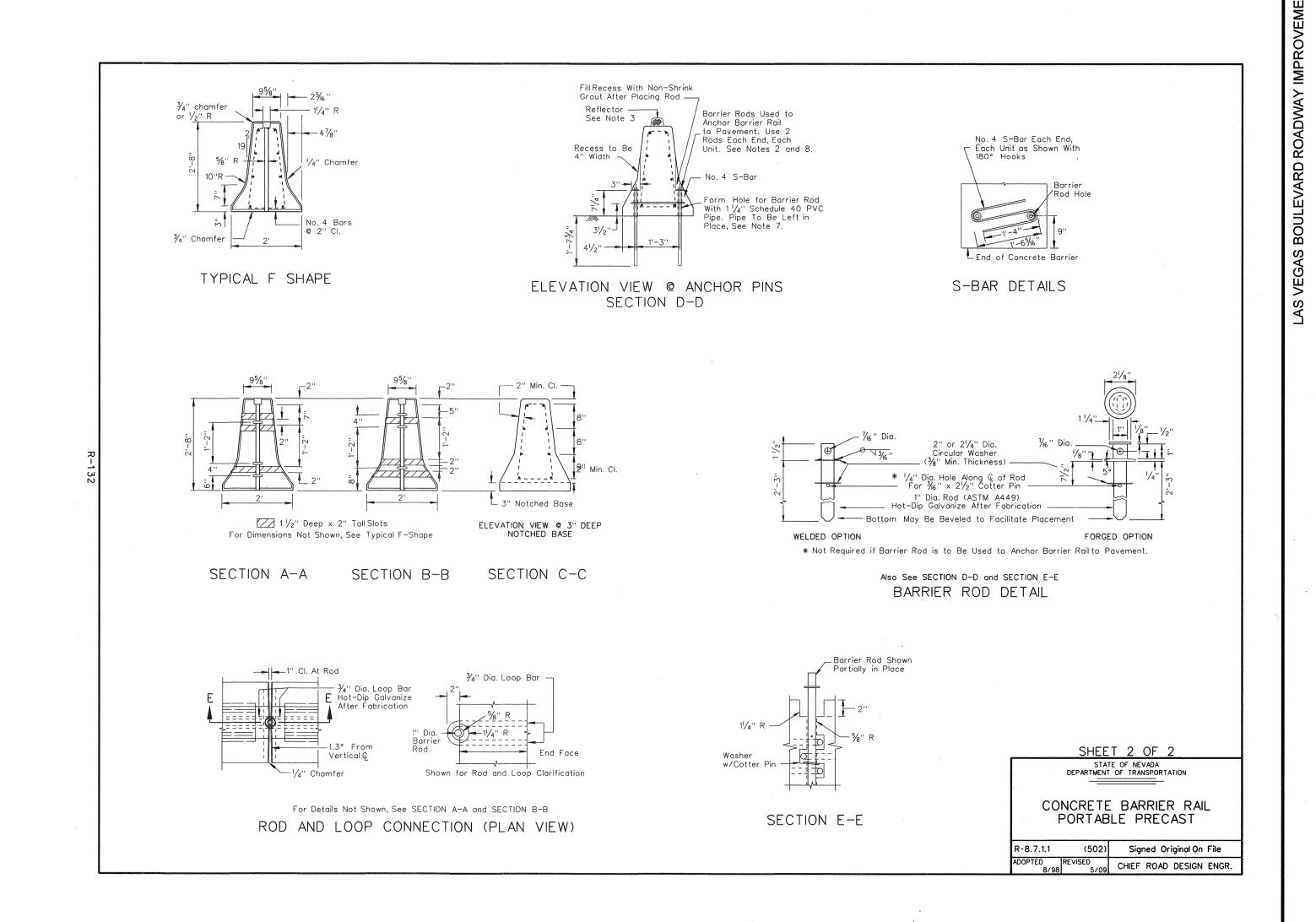
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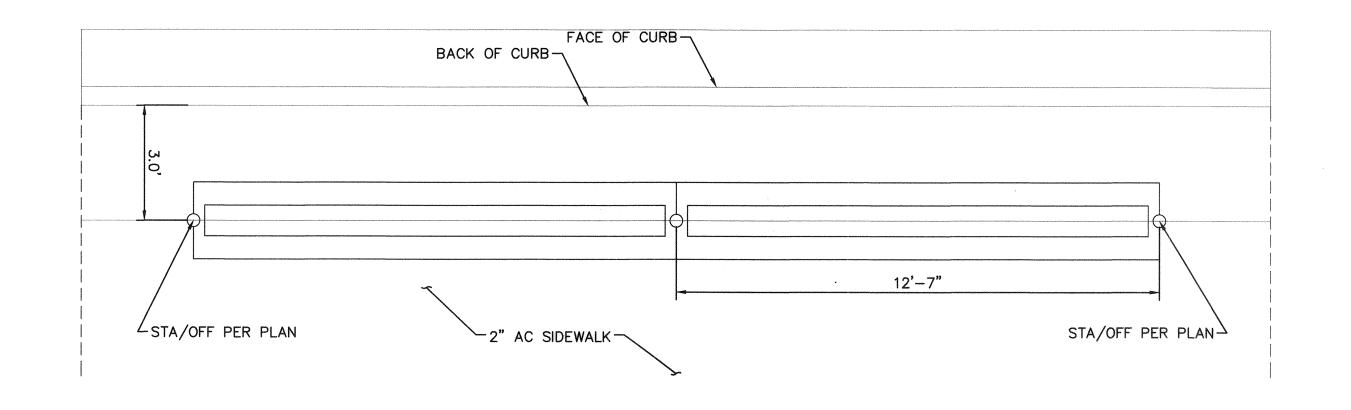


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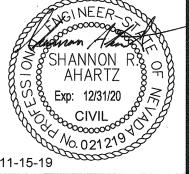
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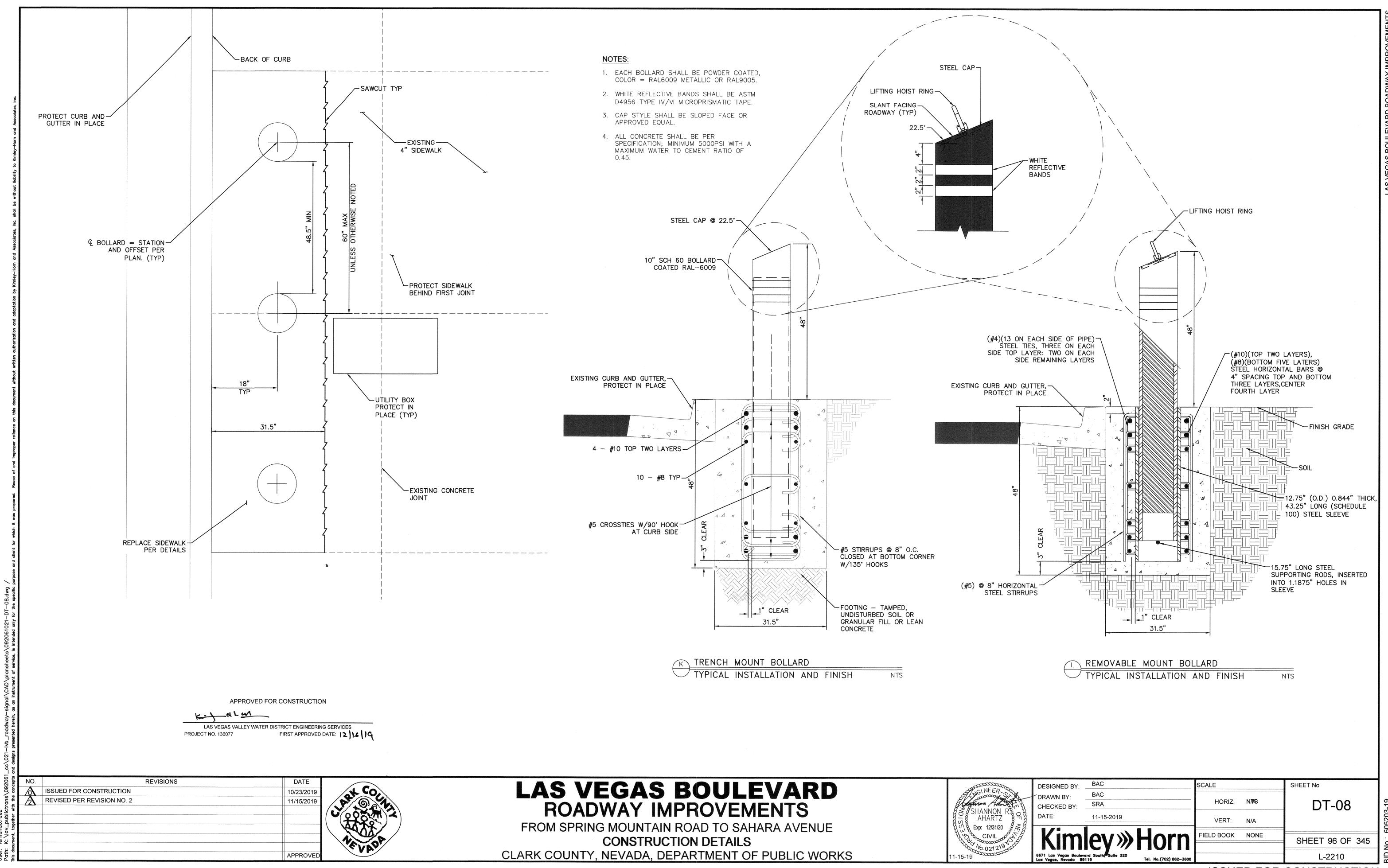
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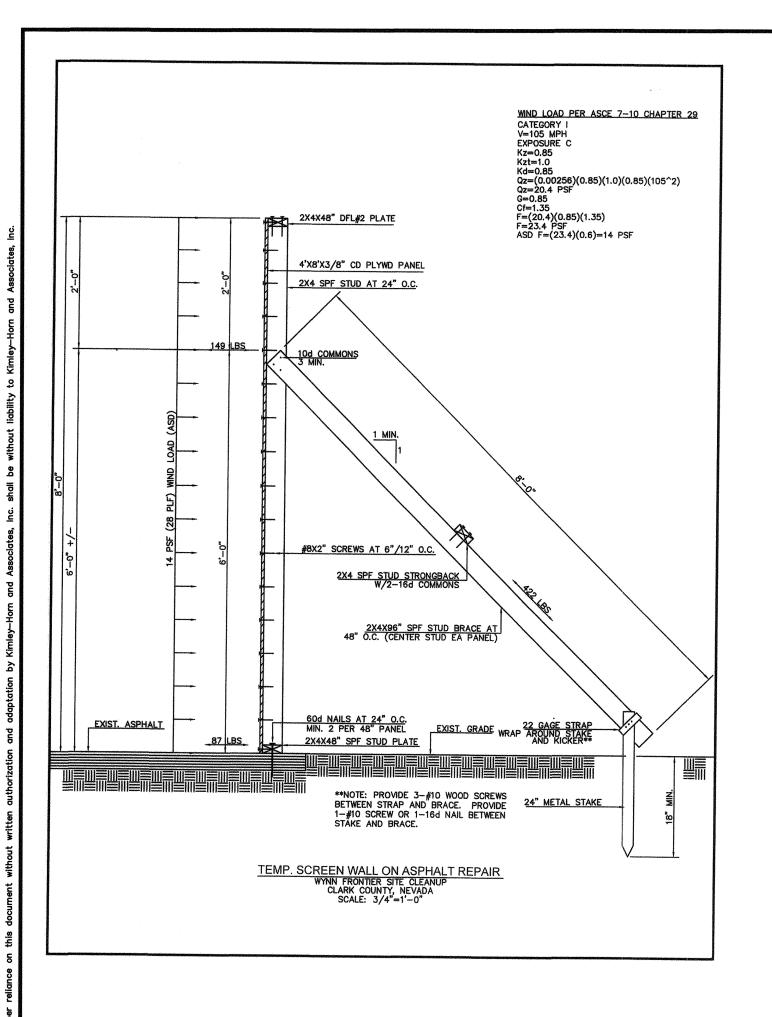
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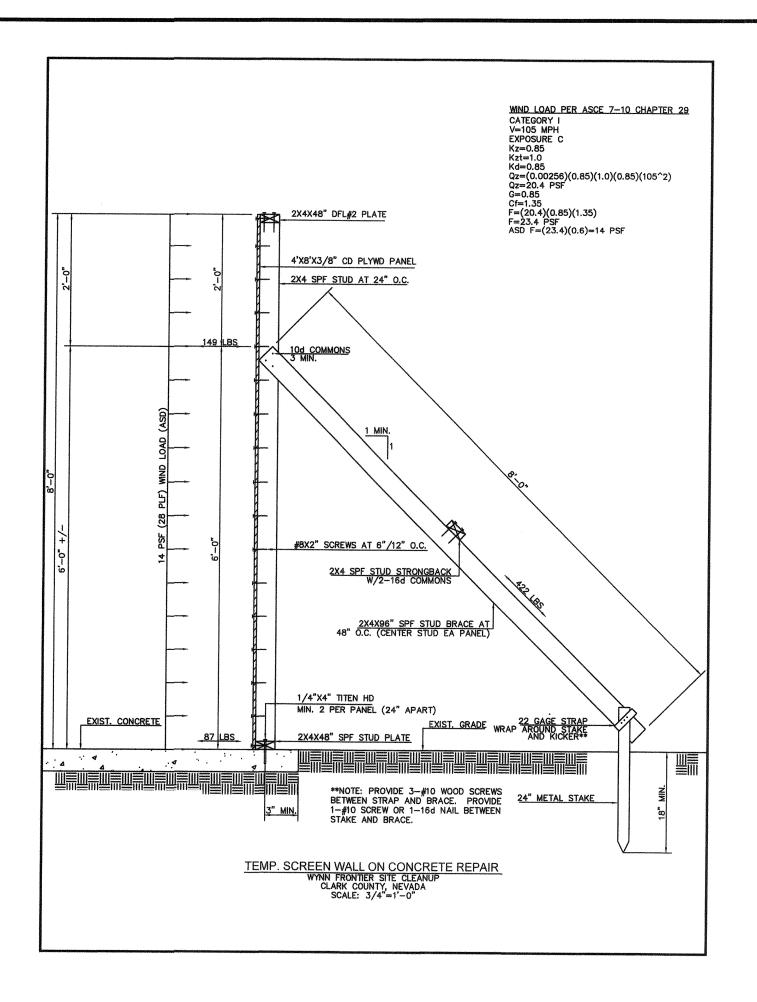
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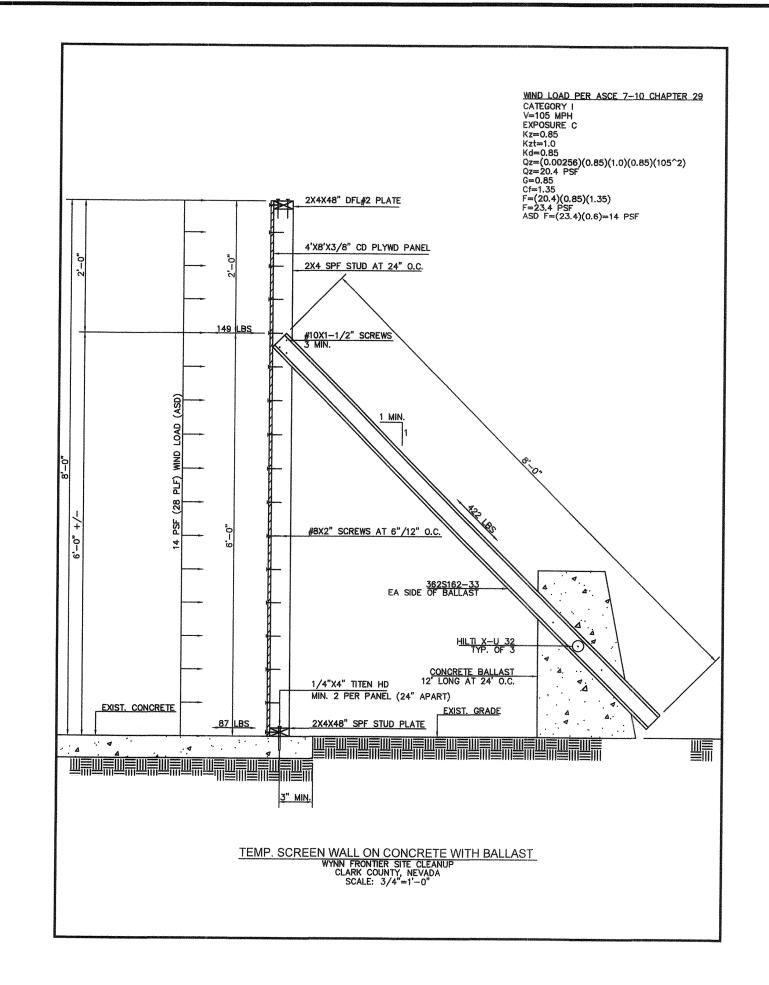
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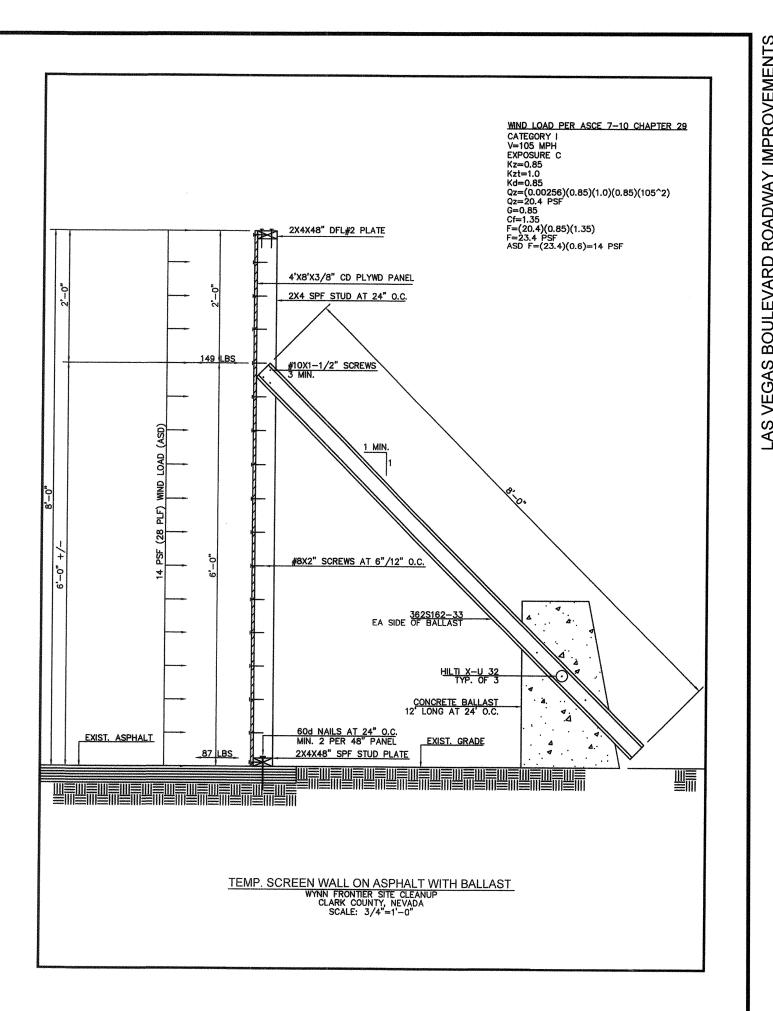
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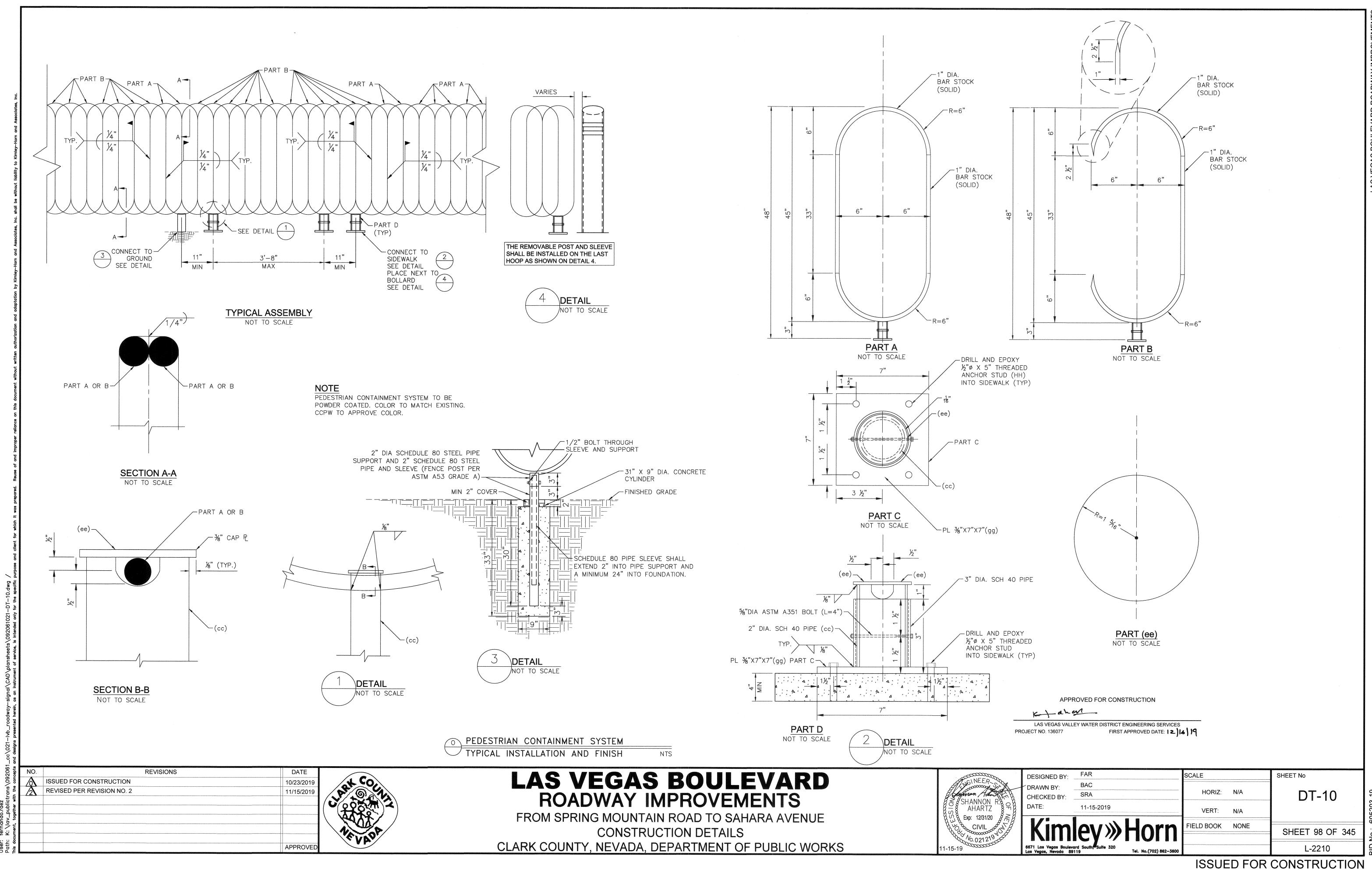
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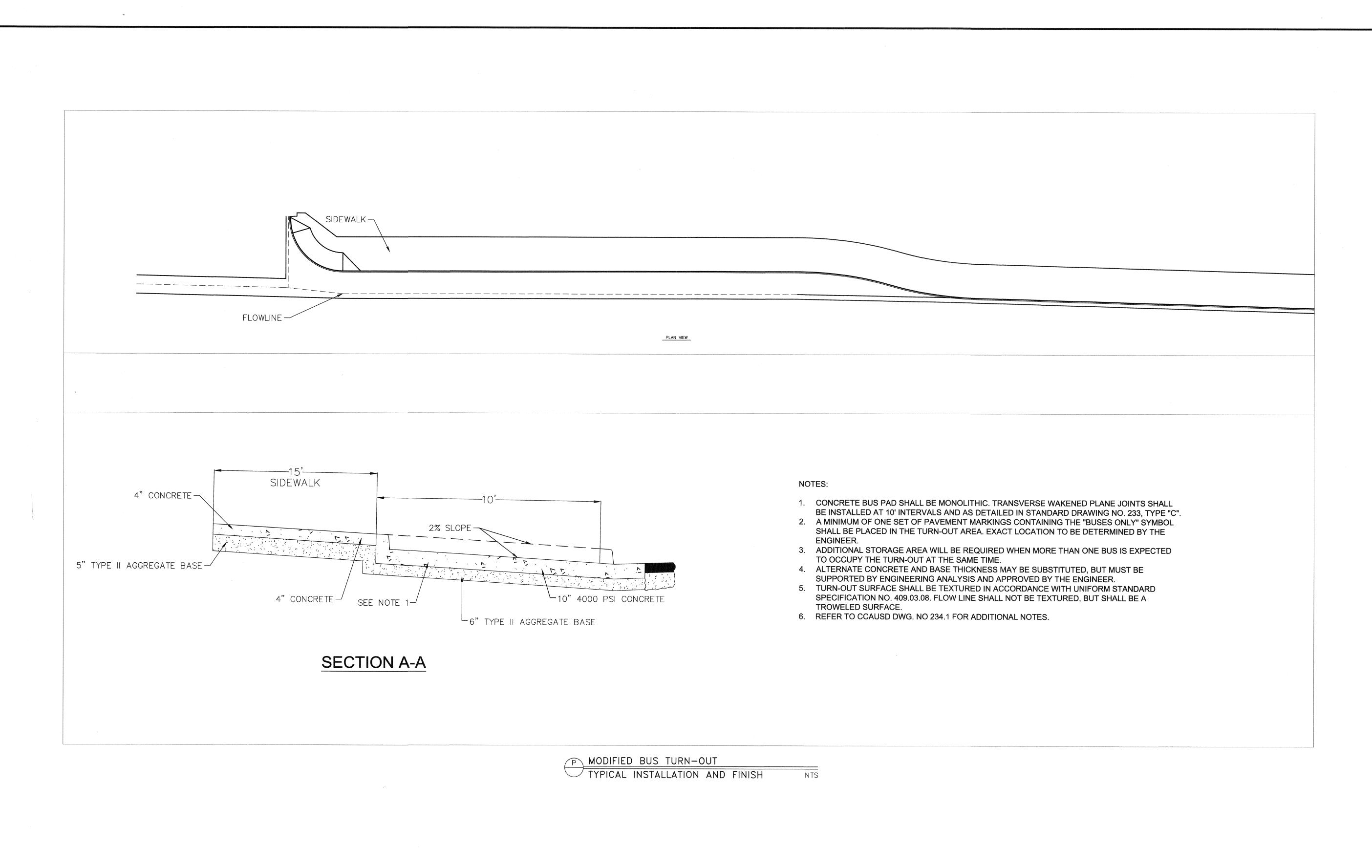
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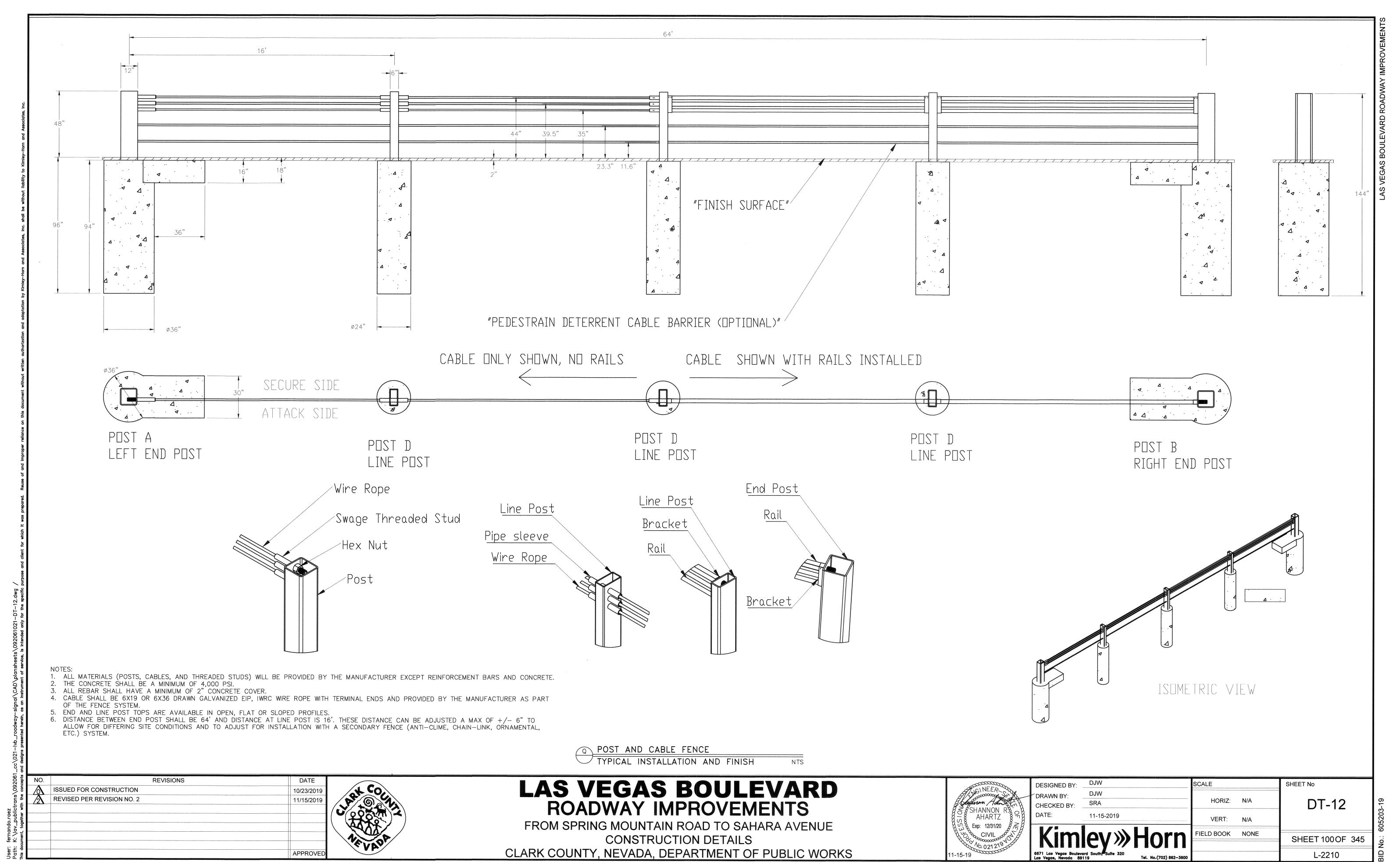
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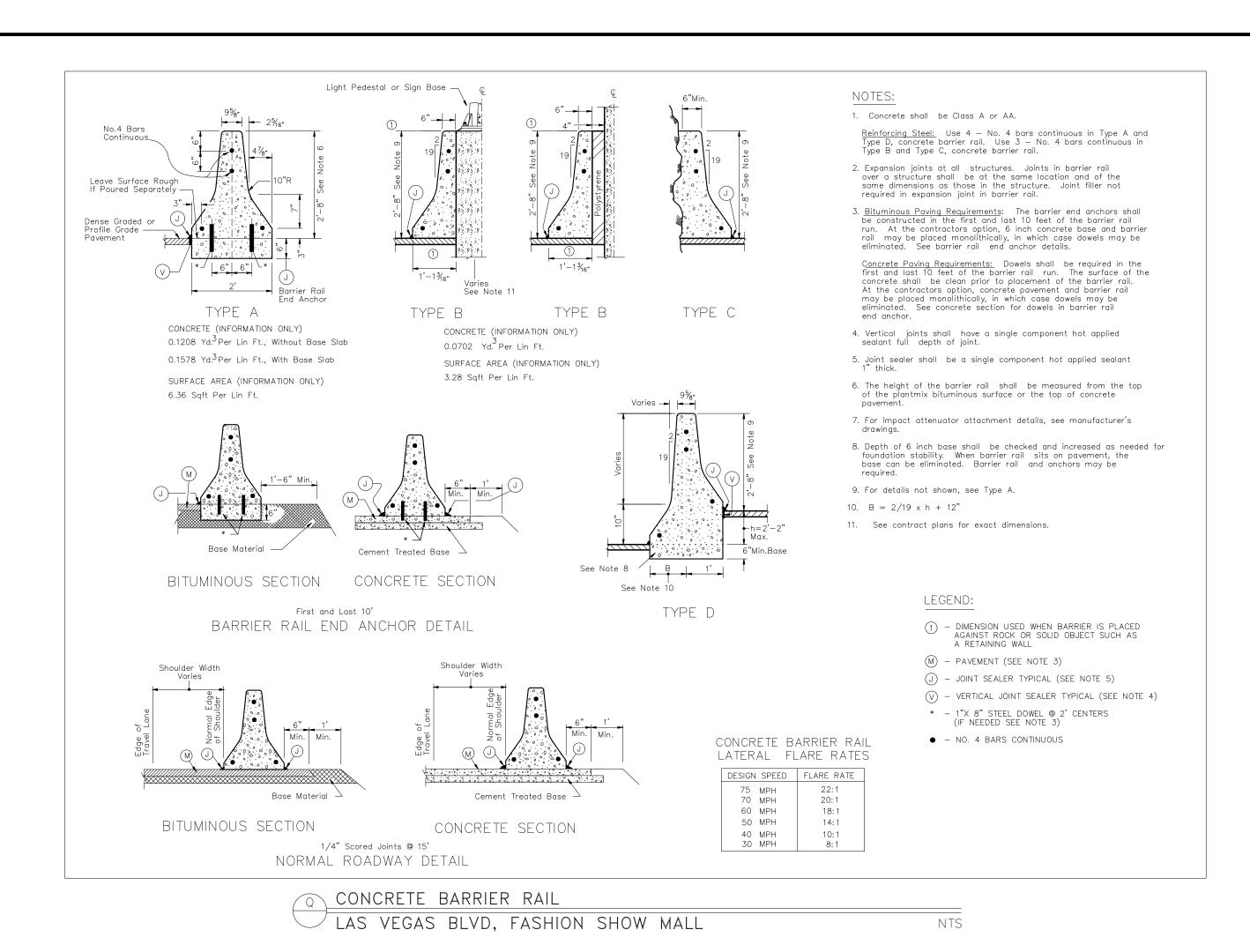
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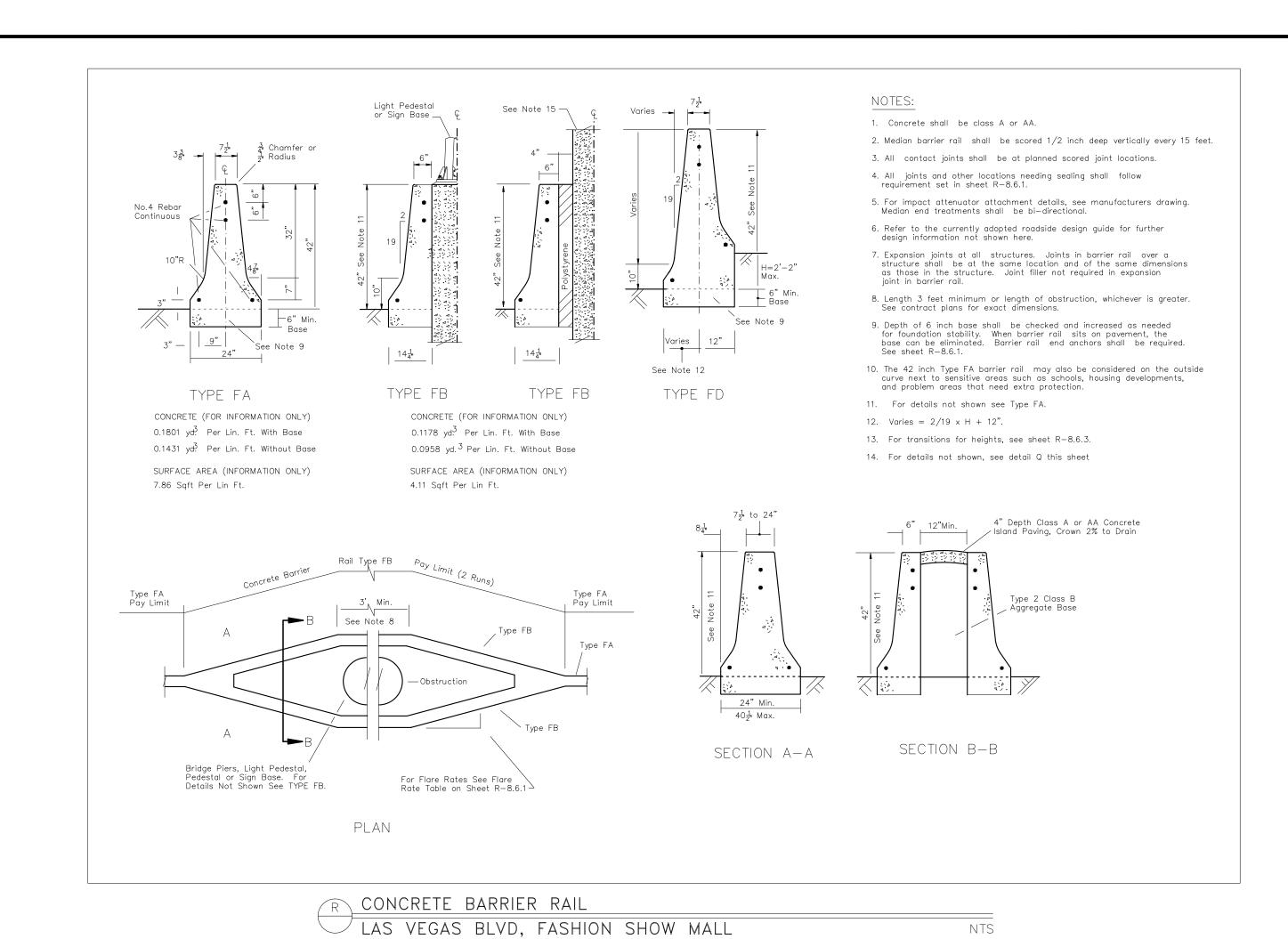
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		ley»Horn	FIELD BOOK	NONE	SHEET 99 OF 345
	6671 Las Vegas Bouleva Las Vegas, Nevada 89	rd South, Suite 320			L-2210







REMOVE EXISTING CURB AND GUTTER BARS SET AT 24" CENTERS — ON ALTERNATE SIDES MATCH EXISTING
PAVEMENT SURFACE EXISTING -LANDSCAPE - REPLACE EXISTING CURB AND GUTTER REPLACE CURB AND GUTTER WITH BARRIER ACTING AS CURB FACE.

S MODIFIED CONCRETE FB BARRIER CROSS SECTION (SEE SHEETB-01) LAS VEGAS BLVD, FASHION SHOW MALL

ISSUED FOR CONSTRUCTION 10/23/2019 REVISED PER REVISION NO. 2 11/15/201 REVISED PER REVISION NO. 8 08/13/202

APPROVE

## LAS VEGAS BOULEVARD ROADWAY IMPROVEMENTS

FROM SPRING MOUNTAIN ROAD TO SAHARA AVENUE CONSTRUCTION DETAILS CLARK COUNTY, NEVADA, DEPARTMENT OF PUBLIC WORKS



			1-000
DESIGNED BY:	FAR	SCALE	
DRAWN BY: CHECKED BY:	SRA FAR	HORIZ:	N/A
DATE:	08-13-2020	VERT:	N/A
Kim	ev»Horn	FIELD BOOK	NONE
6671 Las Vegas Boulevo Las Vegas, Nevada 89	ard South, Suite 320		
Las vegas, Nevada 69	19 161. 10.(702) 502-5000	100115	

1-800-336-9193 DT-13 SHEET100AOF 345 L-2210

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